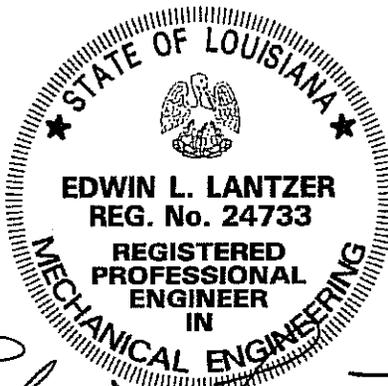


**STATE OF LOUISIANA
DEPARTMENT OF TRANSPORTATION AND
DEVELOPMENT**

CONSTRUCTION PROPOSAL



**STATE PROJECT NO. 007-03-0056
US 61 BARRIER SYSTEM
(LA 50 TO LA 48)
ROUTE US 61
ST. CHARLES PARISH**



Edwin Lantzer
22 APRIL 2009

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NOTICE TO CONTRACTORS (11/08)

Electronic bids and electronic bid bonds for the following project will be downloaded by the Department of Transportation and Development (DOTD) on Wednesday, **May 27, 2009**. **Paper bids and paper bid bonds will not be accepted.** Electronic bids and electronic bid bonds must be submitted through www.bidx.com prior to the electronic bidding deadline. Beginning at 10:00 a.m., all bids will be downloaded and posted online at <http://www.dotd.la.gov/cgi-bin/construction.asp>. No bids are accepted after 10:00 a.m.

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DESCRIPTION: US 61 BARRIER SYSTEM (LA 50 TO LA 48)

ROUTE: US 61

PARISH: ST. CHARLES

LENGTH: 6.82 miles.

TYPE: INSTALLATION OF GUARDRAIL AND ASPHALT STRIP, AND RELATED WORK.

LIMITS: State Project No. 007-03-0056: LOCATED ON ROUTE US 61 FROM ITS JUNCTION WITH ROUTE LA 50 to ITS JUNCTION WITH ROUTE LA 48.

ESTIMATED COST RANGE: \$1,000,000 to \$2,500,000

PROJECT ENGINEER: DUPLANTIS, MICHAEL; P.O. Box 728, Kenner, LA 70063, (504) 465-3473.

PROJECT MANAGER: GUILBEAU, JUSTIN.

Bids must be prepared and submitted in accordance with Section 102 of the 2006 Louisiana Standard Specifications for Roads and Bridges as amended by the project specifications, and must include all information required by the proposal.

NOTICE TO CONTRACTORS (CONTINUED)

Paper plans and/or proposals may be obtained in Room 101-A of the DOTD Headquarters Administration Building, 1201 Capitol Access Road in Baton Rouge, or by contacting the DOTD; Email: sharonknight@dotd.la.gov, Phone (225) 379-1111, FAX: (225) 379-1714, or by written requests sent to the Louisiana Department of Transportation and Development, Project Control Section, P. O. Box 94245, Baton Rouge, LA 70804-9245. Proposals will not be issued later than 24 hours prior to the time set for opening bids. All Addenda, Amendments, Letters of Clarification, and Withdrawal Notices will be posted online. **Paper notices will not be distributed.** Construction proposal information may be accessed via the Internet at www.dotd.la.gov. From the LA DOTD home page, select the following options: **Doing Business with DOTD**, then **Construction Letting Information**. Once the **Construction Letting Information** page appears, find the **Notice to Contractors** box. From the drop down menu, select the appropriate letting date and press the "Go To" button to open the page, which provides a listing of all projects to be let and a **Construction Proposal Documents** link for each project. All project specific notices are found here. **It will be the responsibility of the bidder to check for updates.** If paper copies of the proposal are desired, the proposal cost is \$25.00. Paper copies of the plans are included in the proposal (no additional charge). The purchase price for paper plans and proposals is non-refundable. Additionally, plans and specifications may be seen at the Project Engineer's office or in Room 101-A of the DOTD's Headquarters Administration Building in Baton Rouge. Upon request, the Project Engineer will show the work.

All questions concerning the plans shall be submitted via the Electronic Plans Distribution Center known as **Falcon**. Questions submitted within 96 hours of the bid deadline may not be answered prior to bidding. Falcon may be accessed via the Internet at www.dotd.la.gov. From the home page, select **Doing Business with DOTD** from the left-hand menu, then select **Construction Letting Information** on the pop-up menu. On the Construction Letting Information page, select the link, **DOTD's Plan Room**. Login to Falcon (or request an ID if a first-time user). Once logged in, you will have access to view Project Information, submit a question concerning the project, and view the plans. All submitted questions will be forwarded by email to the Project Manager and the Project Engineer for a response.

The U. S. Department of Transportation (DOT) operates a toll free "Hotline" Monday through Friday, 8:00 a.m. to 5:00 p.m., eastern time. Anyone with knowledge of possible bid rigging, bidder collusion, or other fraudulent activities should call 1-800-424-9071. All information will be treated confidentially and caller anonymity will be respected.

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GENERAL BIDDING REQUIREMENTS (08/06): The specifications, contract and bonds governing the construction of the work are the 2006 Edition of the Louisiana Standard Specifications for Roads and Bridges, together with any supplementary specifications and special provisions attached to this proposal.

Bids shall be prepared and submitted in accordance with Section 102 of the Standard Specifications.

The plans herein referred to are the plans approved and marked with the project number, route and Parish, together with all standard or special designs that may be included in such plans. The bidder declares that the only parties interested in this proposal as principals are those named herein; that this proposal is made without collusion or combination of any kind with any other person, firm, association, or corporation, or any member or officer thereof; that careful examination has been made of the site of the proposed work, the plans, Standard Specifications, supplementary specifications and special provisions above mentioned, and the form of contract and payment, performance, and retainage bond; that the bidder agrees, if this proposal is accepted, to provide all necessary machinery, tools, apparatus and other means of construction and will do all work and furnish all material specified in the contract, in the manner and time therein prescribed and in accordance with the requirements therein set forth; and agrees to accept as full compensation therefore, the amount of the summation of the products of the quantities of work and material incorporated in the completed project, as determined by the engineer, multiplied by the respective unit prices herein bid.

It is understood by the bidder that the quantities given in this proposal are a fair approximation of the amount of work to be done and that the sum of the products of the approximate quantities multiplied by the respective unit prices bid shall constitute gross sum bid, which sum shall be used in comparison of bids and awarding of the contract.

The bidder further agrees to perform all extra and force account work that may be required on the basis provided in the specifications.

The bidder further agrees that within 15 calendar days after the contract has been transmitted to him, he will execute the contract and furnish the Department satisfactory surety bonds.

If this proposal is accepted and the bidder fails to execute the contract and furnish bonds as above provided, the proposal guaranty shall become the property of the Department; otherwise, said proposal guaranty will be returned to the bidder; all in accordance with Subsection 103.04.

MANDATORY ELECTRONIC BIDS AND ELECTRONIC BID BONDS SUBMISSION (10/08): This project requires mandatory electronic bidding. All Specifications, whether Standard, Supplemental or Special Provisions, are hereby amended to delete any references regarding paper bids and the ability to submit paper bid forms.

The contractor shall register online to be placed on the Louisiana Department of Transportation and Development (LA DOTD) prospective bidders list or for information only list.

Modifications to proposal documents will be posted on the Department's website at the following URL address: www.dotd.la.gov/cgi-bin/construction.asp.

LA DOTD shall not be responsible if the bidder cannot complete and submit a bid due to failure or incomplete delivery of the files submitted via the internet.

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COST-PLUS-TIME BIDDING PROCEDURE (A + B METHOD)(08/06): The 2006 Standard Specifications and Supplemental Specifications, as amended elsewhere herein, are further amended as follows:

General. The process for bidding and the award of this project will take into account not only the contract amount bid but also the bidder's stated contract time in which the project will be completed to final acceptance. This method will only be used to determine the successful bidder. It will not be used to determine the award amount nor final payment to the contractor.

Definition of Terms. For this project the following definitions apply:

- (a) Calendar Day – Refer to Subsection 101.03.
- (b) Contract Amount – The summation of the products of the quantities shown in the Schedule of Items multiplied by the unit bid prices.
- (c) Contract Time – The number of calendar days stated in the successful bidders proposal to complete the project to final acceptance as adjusted by authorized extensions.
- (d) Daily Road User Cost – The amount which represents the average daily cost of interference and inconvenience to the road user. The Department has assigned a daily road user cost of \$3000 per calendar day for this project.
- (e) Final Acceptance – Refer to Subsection 105.17(b).

Preparation of Proposal. In addition to all other bidding requirements of the project specifications, the bidder shall state his required completion time in the space provided on the "CONTRACT TIME" form contained elsewhere herein. The proposed completion time shall be based on the construction phases shown in the plans in their respective order and will be a factor used in considering bids for award. The stated number of calendar days required for completion will be the contract time for this project should the bidder be successful. The total number of days stated by the bidder to complete the project shall not exceed the maximum allowable contract time stated on the "CONTRACT TIME" form contained elsewhere herein. Bids not including a contract time, or showing time to completion in excess of the maximum amount will be considered irregular and will be rejected.

Consideration of Bids. After bids are opened and read, they will be compared based on the Total Bid Amount as determined by the following formula. In case of equal total bid amounts between qualified bidders, award will be made to the bidder proposing the lowest contract time.

$$\text{Total Bid Amount} = A + B$$

Where:

A = the contract amount as defined herein.

B = the product of the number of calendar days of contract time stated by the bidder and the daily road user cost contained herein.

Conditional Notice to Proceed/Notice to Proceed. If this A + B project is awarded during the months of September, October or November, the Department will consider issuing a Conditional Notice to Proceed with an expiration date of March 1 of the following calendar year, whereupon a Notice to Proceed will become effective. Such request for delay from the contractor shall be in writing with justification for the delay. If a Conditional Notice to Proceed is issued then any assembly period, as provided in the special provision "Contract Time", is negated.

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Late Completion. Should the contractor fail to complete the project to final acceptance prior to expiration of the contract time, stipulated damages will be charged an amount equal to the daily road user cost stated herein.

MAINTENANCE OF TRAFFIC (11/13/08): Subsection 104.03 of the 2006 Standard Specifications is amended to include the following requirements.

The contractor shall provide for and maintain through and local traffic at all times and shall conduct his operations in such manner as to cause the least possible interference with traffic at junctions with roads, streets and driveways.

SUBLETTING OF CONTRACT (01/83): In accordance with Subsection 108.01 of the Standard Specifications, the following items are designated as "Specialty Items":

- Item 202-02-14500, Removal of Guard Rail
- Item 502-01-00200, Superpave, Asphaltic Concrete, Drives, Turnouts and Miscellaneous
- Item 704-01-02000, Guard Rail (Double Thrie Beam) (3-1 1/2" post spacing)
- Item 704-03-00100, Blocked Out Guard Rail
- Item 704-06-00100, Guard Rail Anchor Sections (Trailing End)
- Item 704-06-00200, Guard Rail Anchor Sections (Trailing End) (Single Thrie Beam)
- Item 704-08-00200, Guard Rail Transitions (Double Thrie Beam)
- Item 704-11-00100, Guard Rail End Treatment (Flared)

PROSECUTION OF WORK (12/08): Subsection 108.04, Prosecution of Work of the Standard Specifications as amended by the supplemental specifications thereto, is further amended as follows.

108.04 PROSECUTION OF WORK.

Subpart (a), General is deleted and the following substituted.

(a) General: The contractor shall provide sufficient materials, equipment and labor to complete the project in accordance with the plans and specifications within the contract time. If the completed work is behind the approved progress schedule, the contractor shall take immediate steps to restore satisfactory progress and shall not transfer equipment or forces from uncompleted work without prior notice to, and approval of, the engineer. Each item of work shall be prosecuted to completion without delay. If prosecution of the work is discontinued for an extended period of time, the contractor shall give the engineer written notice at least 24 hours before resuming operations. The contractor's progress will be determined monthly at the time of each partial estimate, and will be based on the total amount earned by the contractor as reflected by the partial estimate. If the contractor's progress is behind more than 20 percent behind the elapsed contract time, the contractor may be notified that he is not prosecuting the work in an acceptable manner. If requested by the Department the contractor must meet with and provide the project engineer with an acceptable written plan which details how the contractor will re-gain lost progress and prosecute remaining work. If the contractor's progress is more than 30 percent behind the elapsed contract time, the contractor and the surety will be notified that he is not prosecuting the work in an acceptable manner. The contractor must meet with and provide the project engineer with an acceptable written plan which details how the contractor will re-gain lost progress and prosecute remaining work.

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Subpart (b), Disqualification is deleted and the following substituted.

(b) Disqualification: A contractor who is in default in accordance with Subsection 108.09(a)(1) of and progress is deficient by 10 percent or more shall be immediately disqualified. The contractor shall remain disqualified until the project has received a final inspection and has been recommended for final acceptance. Should the surety or the Department take over prosecution of the work, the contractor shall remain disqualified for a period of one year from the completion of the project, unless debarment proceedings are instituted.

During the period of disqualification, the contractor will not be permitted to bid on contracts nor be approved as a subcontractor on contracts. Any bid submitted by the contractor during the period of disqualification will not be considered and will be returned.

PAYMENT ADJUSTMENT (12/08): Section 109, Measurement and Payment of the 2006 Standard Specifications and the supplemental specifications thereto, is amended to add the following.

This project is designated for payment adjustment for asphalt cements and fuels in accordance with Subsection 109.09 as follows.

109.09 PAYMENT ADJUSTMENT (ASPHALT CEMENTS AND FUELS).

(a) General: Payment for contract items indicated herein will be adjusted to compensate for cost differentials of Performance Graded (PG) asphalt cements, gasoline, and diesel fuel when such costs increase or decrease more than 5 percent from the Department's established base prices for these items. The base price indices for asphalt cements and fuels will be the monthly price indices in effect at the time bids are opened for the project. The base price indices for asphalt cements will be as stated in paragraph (b) below. The base price index for fuels will be as stated in paragraph (c) below.

Payment adjustments will be made each monthly estimate period when a price index for this period varies more than 5 percent from its respective base price index. The monthly price indices to be used with each monthly estimate will be the price indices for the month in which the estimate period begins.

If the project is placed in default, payment adjustments will be based on the monthly price indices used for the last monthly estimate period prior to the project being placed in default, unless a monthly price index decreases in which case the lower monthly price index will be used.

If it is determined after completion of work on any eligible item that the total quantity paid to date must be adjusted to reflect more accurate quantity determinations, the Department will prorate the additional quantity to be added or subtracted over all previous estimate periods in which the item of work was performed in order to determine additional payment adjustments. If payment adjustments were made during any of these partial estimate periods, this added or subtracted quantity that has been prorated will likewise have payment adjustments calculated and included.

(b) Performance Graded (PG) Asphalt Cements: The base price index will be the monthly price index in effect at the time of bid opening as shown elsewhere herein. The monthly price indices will be the average, excluding the extreme outliers, of the unit prices for PG 64-22, the average, excluding the extreme outliers, of the unit prices for PG 70-22m, and the average,

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excluding the extreme outliers, of the unit prices for PG 76-22m. The monthly prices for each of these asphalt materials will be F.O.B. refinery or terminal as determined from the quoted prices effective on the first calendar day of each month from suppliers of these materials. Suppliers considered are those who have requested to participate in the liquid asphalt index determination and have supplied materials on DOTD projects within the past twelve months. These suppliers and materials shall be listed on the Department's Qualified Products List (QPL 41) and must be marketed in Louisiana. For Asphalt Cements not listed above, the following shall be considered equivalent for payment adjustments:

Pay Item Equivalents Eligible for Asphalt Pay Adjustment

Performance Graded Asphalt Cement	Equivalent PG Asphalt Cement for Payment Adjustment
PG 58-28	PG 64-22
PG 64-22	PG 64-22
PG 70-22m	PG 70-22m
PG 76-22m	PG 76-22m
PG 82-22rm	PG 64-22

Payment adjustments will be made in accordance with the following formulas:

If Monthly Price Index exceeds Base Price Index,

$$P_a = (A - 1.05B) \times C \times D \times (1.00 + T)$$

If Base Price Index exceeds Monthly Price Index,

$$P_a = (0.95B - A) \times C \times D \times (1.00 + T)$$

Where:

- P_a = Price adjustment (increase or decrease) for asphalt cement.
- A = Monthly Price Index for respective PG 64-22, PG 70-22m, or PG 76-22m in dollars per ton/megagram.
- B = Base Price Index for respective PG 64-22, PG 70-22m, or PG 76-22m in dollars per ton/megagram.
- C = Tons/megagrams of asphaltic concrete.
- D = Percent of respective asphalt cement, per job mix formula, in decimals.
- T = Louisiana sales tax percentage, in decimals.

(Note: Local tax is not considered)

The engineer will furnish the weights (mass) of asphaltic concrete placed during the monthly estimate period with the respective asphalt cement content, excluding the asphalt content in reclaimed asphaltic pavement (RAP) as per job mix formula. If the asphalt cement content changes during the estimate period, the respective weight (mass) of asphaltic concrete produced at each cement content will be reported.

All contract pay items using PG 58-28, PG 64-22, PG 70-22m, PG 76-22m, and PG 82-22rm shall be eligible for payment adjustments of asphalt materials; except no payment adjustment will be made for contract pay items under Subsection 510-01, "Pavement Patching", Section 507, "Asphaltic Surface Treatment", nor for any emulsions of cutbacks.

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Item 510-02, Pavement Widening, and all contract pay items under Sections 502 and 508, will be eligible for payment adjustments of asphalt materials. No payment adjustment will be made for other asphalt materials, including emulsions and cutbacks.

The base price indices for asphalt cements and fuels will be posted on the DOTD internet website before the 10th calendar day of each month at the following URL: www.dotd.louisiana.gov/lettings/lac_price_index/priceindices.asp.

(c) Fuels: The base price index for this project will be the monthly price index in effect when bids are opened for the project. The monthly price index will be the minimum price quotations for unleaded gasoline and No. 2 diesel fuel listed for the New Orleans area in *Platt's Oilgram and Price Report* effective on the first calendar day of each month.

Payment adjustment will be made in accordance with the following formulas:

If Monthly Price Index exceeds Base Price Index,

$$P_a = (A - 1.05B) \times Q \times F$$

If Base Price Index exceeds Monthly Price Index,

$$P_a = (0.95B - A) \times Q \times F$$

Where:

P_a	=	Price adjustment.
A	=	Monthly Price Index in dollars per gallon/liter.
B	=	Base Price Index in dollars per gallon/liter.
Q	=	Pay Item Quantity (Pay Units).
F	=	Fuel Usage Factor Gal (L)/Pay Unit.

The following is a listing of contract pay items that are eligible for payment adjustment and the fuel usage factors that will be used in making such adjustment. Contract items that expand the items listed herein by use of letter or number designations are also eligible for fuel price adjustments; for example:

Item 601-01-G, Portland Cement Concrete Pavement 8 inches (200 mm) thick.

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**ELIGIBLE CONTRACT PAY ITEMS & FUEL USAGE FACTORS FOR FUEL
PAYMENT ADJUSTMENT⁷**

ITEM NO.	PAY ITEM	UNITS	MIN. ORIGINAL CONTRACT QUANTITY FOR PAY ADJUSTMENT	FUEL USAGE FACTORS	
				Diesel ²	Gasoline
203-01 ¹	General Excavation	gal/cu yd	10,000 cu yd	0.29	0.15
203-02	Drainage Excavation	gal/cu yd	10,000 cu yd	0.29	0.15
203-03 ¹	Embankment	gal/cu yd	10,000 cu yd	0.29	0.15
203-04	Nonplastic Embankment	gal/cu yd	10,000 cu yd	0.29	0.15
203-07	Borrow (Vehicular Measurement)	gal/cu yd	10,000 cu yd	0.29	0.15
301-01	Class I Base Course	gal/cu yd	3,000 cu yd	0.88	0.57
301-02	Class I Base Course (" Thick)	gal/sq yd	50,000 sq yd	0.04	0.03
302-01	Class II Base Course	gal/cu yd	3,000 cu yd	0.88	0.57
302-02	Class II Base Course (" Thick)	gal/sq yd	50,000 sq yd	0.04	0.03
303-01	In-Place Cement Stabilized Base Course	gal/sq yd	50,000 sq yd	0.04	0.03
304-02	Lime Treatment (Type B)	gal/sq yd	50,000 sq yd	0.04	0.03
304-03	Lime Treatment (Type C)	gal/sq yd	50,000 sq yd	0.04	0.03
304-04	Lime Treatment (Type D)	gal/sq yd	50,000 sq yd	0.04	0.03
305-01	Subgrade Layer (" Thick)	gal/sq yd	50,000 sq yd	0.04	0.03
308-01	In-Place Cement Treated Base Course	gal/sq yd	50,000 sq yd	0.04	0.03
401-01	Aggregate Surface Course (Net Section)	gal/cu yd	3,000 cu yd	0.88	0.57
401-02	Aggregate Surface Course (Adjusted Vehicular Measurement)	gal/cu yd	3,000 cu yd	0.88	0.57
502-01	Superpave Asphaltic Concrete	gal/ton	1000 ton	2.40 ³	0.2
502-02	Superpave Asphaltic Concrete	gal/cu yd	500 cu yd	4.80 ⁴	0.4
502-03	Superpave Asphaltic Concrete (" Thick)	gal/sq yd	10,000 sq yd	0.13 ^{5,6}	0.01 ⁶
508-01	Asphaltic Concrete (SMA)	gal/ton	1000 ton	2.40 ³	0.2
510-02	Pavement Widening	gal/sq yd	3,000 sq yd	0.86	0.24
601-01	Portland Cement Concrete Pavement (" Thick)	gal/sq yd	15,000 sq yd	0.11	0.15

- 1 If project has both 203-01 & 203-03, only the item with larger quantity is eligible.
- 2 For fuel adjustment purposes, the term "diesel" shall represent No. 2 or No. 4 fuel oils or any of the liquified petroleum gases, such as propane or butane.
- 3 If natural gas or coal is used instead of diesel for aggregate drying and heating the fuel usage factor shall be 1.67 gal/ton.
- 4 If natural gas or coal is used instead of diesel for aggregate drying and heating the fuel usage factor shall be 13.34 gal/cu yd.
- 5 If natural gas or coal is used instead of diesel for aggregate drying and heating the fuel usage factor shall be 0.09 gal/sq yd.
- 6 Per inch of thickness.
- 7 No fuel adjustment will be allowed for waste oil.

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**ELIGIBLE CONTRACT PAY ITEMS & FUEL USAGE FACTORS FOR FUEL
PAYMENT ADJUSTMENT (METRIC)⁷**

ITEM NO.	PAY ITEM	UNITS	MIN. ORIGINAL CONTRACT QUANTITY FOR PAY ADJUSTMENT	FUEL USAGE FACTORS	
				Diesel ²	Gasoline
203-01 ¹	General Excavation	l/m ³	7,600 m ³	1.44	0.74
203-02	Drainage Excavation	l/m ³	7,600 m ³	1.44	0.74
203-03 ¹	Embankment	l/m ³	7,600 m ³	1.44	0.74
203-04	Nonplastic Embankment	l/m ³	7,600 m ³	1.44	0.74
203-07	Borrow (Vehicular Measurement)	l/m ³	7,600 m ³	1.44	0.74
301-01	Class I Base Course	l/m ³	2,300 m ³	4.36	2.82
301-02	Class I Base Course (mm Thick)	l/m ²	41,800 m ²	0.18	0.14
302-01	Class II Base Course	l/m ³	2,300 m ³	4.36	2.82
302-02	Class II Base Course (mm Thick)	l/m ²	41,800 m ²	0.18	0.14
303-01	In-Place Cement Stabilized Base Course	l/m ²	41,800 m ²	0.18	0.14
304-02	Lime Treatment (Type B)	l/m ²	41,800 m ²	0.18	0.14
304-03	Lime Treatment (Type C)	l/m ²	41,800 m ²	0.18	0.14
304-04	Lime Treatment (Type D)	l/m ²	41,800 m ²	0.18	0.14
305-01	Subgrade Layer (mm Thick)	l/m ²	41,800 m ²	0.18	0.14
308-01	In-Place Cement Stabilized Base Course	l/m ²	41,800 m ²	0.18	0.14
401-01	Aggregate Surface Course (Net Section)	l/m ³	2,300 m ³	4.36	2.82
401-02	Aggregate Surface Course (Adjusted Vehicular Measurement)	l/m ³	2,300 m ³	4.36	2.82
502-01	Superpave Asphaltic Concrete	l/Mg	900 Mg	10.01 ³	0.83
502-02	Superpave Asphaltic Concrete	l/m ³	400 m ³	23.77 ⁴	1.98
502-03	Superpave Asphaltic Concrete (mm Thick)	l/m ²	8,400 m ²	0.59 ^{5,6}	0.45 ⁶
508-01	Asphaltic Concrete (SMA)	l/Mg	900 Mg	10.01 ³	0.83
510-02	Pavement Widening	l/m ²	2,500 m ²	3.89	1.09
601-01	Portland Cement Concrete Pavement (mm Thick)	l/m ²	12,500 m ²	0.5	0.68

- 1 If project has both 203-01 & 203-03, only the item with larger quantity is eligible.
- 2 For fuel adjustment purposes, the term "diesel" shall represent No. 2 or No. 4 fuel oils or any of the liquified petroleum gases, such as propane or butane.
- 3 If natural gas or coal is used instead of diesel for aggregate drying and heating the fuel usage factor shall be 6.97 l/mg.
- 4 If natural gas or coal is used instead of diesel for aggregate drying and heating the fuel usage factor shall be 16.53 l/m³.
- 5 If natural gas or coal is used instead of diesel for aggregate drying and heating the fuel usage factor shall be 0.41 l/m².
- 6 Per mm of thickness.
- 7 No fuel adjustment will be allowed for waste oil.

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SUPERPAVE ASPHALTIC CONCRETE MIXTURES (11/08): Section 502, Superpave Asphaltic Concrete Mixtures of the 2006 Standard Specifications as amended by the supplemental specifications thereto, is further amended as follows.

Subsection 502.04, Job Mix Formula Validation.

Delete the first sentence of the sixth paragraph and substitute the following.

A JMF is considered validated if the following parameters are 71 percent within limits of the JMF and meet the specifications requirements.

Subsection 502.05, Plant Quality Control.

Delete the first paragraph and substitute the following.

For quality control purposes, the contractor shall obtain a minimum of two (2) samples of mixture from each subplot using a stratified random sampling approach. Test results for theoretical maximum specific gravity (G_{mm}) and measured bulk specific gravity (G_{mb}) at N_{max} and percent G_{mm} at $N_{initial}$, on samples of each subplot shall be reported. Control charts may be requested by the engineer if mixture problems develop. Quality control gyratory samples may be aged or unaged at the contractor's option, but the method chosen shall be used consistently throughout the project. If aged samples are used, report the measured G_{mb} at N_{max} . If unaged samples are used, report the estimated G_{mb} at N_{max} . One loose mix sample shall be taken from each subplot after placement of the mix in the truck. The mix shall be tested by the contractor at the plant for aggregate gradation, asphalt content and percent crushed aggregate. The mix shall be tested in accordance with DOTD TR 309, TR 323 and TR 306. The lot average and standard deviation shall be determined for aggregate gradation and asphalt content. The percent within limits (PWL) shall be determined on the Nos. 8 and 200 (2.36 mm and 75 μ m) sieves and for G_{mm} . Corrective action shall be taken if these parameters fall below 71 PWL. For each lot, the contractor shall report all quality control data to the DOTD Certified Plant Technician. The full range of gradation mix tolerances will be allowed even if they fall outside the control points. The District Laboratory Engineer may require re-validation of the mix when the average of the Quality Control data indicates non-compliance with the specified limits or tolerances.

Subsection 502.15, Measurement.

Subheading (c), Surface Tolerance Incentive Measurement.

Delete the first paragraph and substitute the following.

At the completion of construction of the project, an independent certified profiler such as that of a private company or the Materials and Testing Section, approved by the Department, shall be used to measure a continuous profile from the start station to the end station of the construction project for the purpose of determining qualification for incentive pay under Subsection 502.16(e). Bridges and 300 feet (90 m) on each end of the bridge will be excluded from measurements for surface tolerance incentive pay.

Delete Table 502-7A, Payment Adjustment Schedule for Plant Acceptance and substitute the following.

**STATE PROJECT NO. 007-03-0056
SPECIAL PROVISIONS**

**Table 502-7A
Payment Adjustment Schedule for Plant Acceptance**

Air Voids PWL (90 AQL)	Percent Payment
71-100	100
61-70	90
51-60	80
≤50	50 or Remove ¹

¹At the option of the Department after investigation.

Delete Table 502-7B, Payment Adjustment Schedule for Roadway Density and substitute the following.

**Table 502-7B
Payment Adjustment Schedule for Roadway Density**

Roadway Density PWL (90 AQL)	Percent Payment
99-100	102
81-98	100
71-80	95
51-70	80
≤50	50 or Remove ¹

¹At the option of the Department after investigation.

Delete Table 502-8A, Payment Adjustment Schedules for Longitudinal Surface Tolerance, Maximum International Roughness Index, inches per mile (mm per km) and substitute the following.

**STATE PROJECT NO. 007-03-0056
SPECIAL PROVISIONS**

**Table 502-8A
Payment Adjustment Schedules for Longitudinal
Surface Tolerance, Maximum International Roughness Index,
inches per mile (mm per km)**

Percent of Contract Unit Price (by Sublot) ¹	102% ²	100%	90%	80%	50% or Remove ³
Category A All Interstates, Multi-Lift New Construction and Overlays of More than two Lifts	<45 (<710)	<65 (<1030)	65-75 (1030-1180)	NA	>75 (>1180)
Category B One or Two Lift Overlays Over Cold Planed Surfaces, and Two-Lift Overlays Over Existing Surfaces ⁴	<55 (<870)	<75 (<1180)	75-89 (1180-1400)	NA	>89 (>1400)
Category C Single-Lift Overlays Over Existing Surfaces ⁴	N/A	<85 (<1340)	85-95 (1340-1500)	>95-110 (>1500-1740)	>110 (>1740)
Longitudinal Surface Tolerance Incentive Pay, Final Completion, Average of All Travel Lanes ⁵	≤ 45 (≤ 710)				

¹Or portion of sublot placed on the project.

²Maximum payment for sublots with exception areas, exclusions or grinding is 100 percent, unless the excluded area is a bridge end.

³At the option of the engineer.

⁴ Existing surfaces include reconstructed bases without profile grade control.

⁵Only Category A projects are eligible for incentive. However, any grinding except within 300 feet (90 m) of a bridge end will cause the roadway to be ineligible for surface tolerance incentive pay. Measurements must be verified by an independent entity.

Delete Table 502-8B, Individual Wheelpath Deficient Area Limits, Maximum International Roughness Index, Inches per Mile (mm per km) and substitute the following.

**Table 502-8B
Individual Wheelpath Deficient Area Limits
Maximum International Roughness Index, inches per mile (mm per km)**

Any 0.05 Mile (0.08 km) Segment	Wearing Course	Binder Course
Category A	89 (1400)	130 (2050)
Category B	99 (1560)	150 (2370)
Category C	N/A	N/A

TEMPORARY TRAFFIC CONTROL (03/09): Section 713 of the 2006 Standard Specifications and the Supplemental Specifications is amended as follows:

Subsection 713.04, Temporary Signs and Barricades, is amended to include the following:

(d) Project Signs: The contractor shall furnish, install, maintain, and upon completion of the project remove "project signs" in accordance with the following requirements.

**STATE PROJECT NO. 007-03-0056
SPECIAL PROVISIONS**

Project signs shall conform to the requirements of Section 713 and the project sign detail contained elsewhere herein. Shop drawings will be furnished to the successful bidder by contacting the Department's Traffic Services Sign Shop at (225) 935-0121 or (225) 935-0142.

Project signs shall be required at the beginning and end of the project and shall follow sign G-20-1, "Road Work Next 'X' Miles", or as directed by the engineer.

Payment for project signs shall be included in the contract unit price for Item 713-01 Temporary Signs and Barricades.

ASPHALT MATERIALS AND ADDITIVES (04/08): Section 1002 of the 2006 Standard Specifications and the supplemental specifications thereto is amended as follows.

Subsection 1002.02, Asphalt Material Additives is amended as follows.

Table 1002-1, Performance Graded Asphalt Cements is deleted and the following substituted.

NS SPECIAL GUARD RAIL ANCHOR SECTION (04/01): This item consists of constructing the special guard rail anchor section as shown on the plans, in accordance with Section 704 of the Standard Specifications, and as directed.

Construction of special guard rail anchor section will be paid for at the contract unit price per each which includes furnishing all materials, labor, tools and equipment necessary to complete the item.

Payment will be made under:

<u>Item No.</u>	<u>Pay Item</u>	<u>Pay Unit</u>
NS-700-00240	Special Guard Rail Anchor Section	Each

NS GUARD RAIL BARRIER MARKER: This item consists of furnishing and installing guardrail barrier markers in accordance with these specifications, as detailed in plans and as directed by the Project Engineer.

MATERIALS: Markers shall be Filtrona Guardrail Barrier Markers, part no. 8004228302 as manufactured by Filtrona Extrusion/Davidson Traffic Control Products, or approved equal. The marker shall be constructed of a UV-stabilized, high-impact thermo plastic alloy. The markers shall be of the type fastened to the guard rail post using galvanized nails, screws, or bolts. The materials used to fasten the reflectors shall be as specified by the manufacturer. The markers shall have reflective sheeting with a minimum area of 15 square inches. The reflective sheeting shall be Type III High Intensity conforming to Sections 729 and 1015 of the *Louisiana Standard Specifications for Roads and Bridges* and be an approved product listed on QPL 13.

CONSTRUCTION REQUIREMENTS: The contractor shall comply with manufacture's installation procedures, plan details, specifications herein, and referenced Sections of the *Louisiana Standard Specifications for Roads and Bridges*.

MEASUREMENT: Measurement will be made by the unit, per each.

**STATE PROJECT NO. 007-03-0056
SPECIAL PROVISIONS**

PAYMENT: Payment for Guardrail Barrier Markers, including all materials, tools, equipment, labor and incidentals necessary to complete the work will be made at the contract unit price, per each.

Payment will be made under:

<u>Item No.</u>	<u>Pay Item</u>	<u>Pay Unit</u>
NS-729-00002	Guard Rail Barrier Marker	Each

CONTRACT TIME (03/05): The entire contract shall be completed in all details and ready for final acceptance in accordance with Subsection 105.17(b) within the time specified by the contractor, which shall not exceed the maximum allowable contract time stated on the "Contract Time" form contained elsewhere herein.

Prior to assessment of contract time, the contractor will be allowed 30 calendar days from the date stipulated in the Notice to Proceed to commence with portions of the contract work including but not limited to assembly periods, preparatory work for materials fabrications such as test piles, or other activities which hinder progress in the beginning stages of construction. Prior to issuance of the Notice to Proceed, the Department will consider extending the assembly period, upon written request from the contractor justifying the need for additional time.

The contractor shall be responsible for maintenance of traffic from the beginning of the assembly period. During the assembly period, the contractor will be allowed to do patching and other maintenance work necessary to maintain the roadway with no time charges when approved by the engineer.

If the contractor begins regular construction operations prior to expiration of the assembly period, the assessment of contract time will commence at the time construction operations are begun.

The contractor is directed to the special provisions and the plans for any restrictions that may affect work schedules.

LOUISIANA
DEPARTMENT OF TRANSPORTATION AND DEVELOPMENT
SUPPLEMENTAL SPECIFICATIONS
(FOR 2006 STANDARD SPECIFICATIONS)

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**LOUISIANA
DEPARTMENT OF TRANSPORTATION AND DEVELOPMENT
SUPPLEMENTAL SPECIFICATIONS**

The 2006 Louisiana Standard Specifications for Roads and Bridges and supplemental specifications thereto are amended as follows.

PART I – GENERAL PROVISIONS

SECTION 101 – GENERAL INFORMATION, DEFINITIONS, AND TERMS:

Subsection 101.03 – Definitions (07/07), Pages 3 – 13).

Delete the definition for “Proposal/Bid Guaranty” and substitute the following.

Proposal / Bid Guaranty. The required security furnished with a bid. The only form of security acceptable is a Bid Bond.

SECTION 102 – BIDDING REQUIREMENTS:

Subsection 102.09 – Proposal / Bid Guaranty (07/07), Page 19.

Delete the contents of this subsection and substitute the following.

PROPOSAL/BID GUARANTY. Each bid shall be accompanied by a proposal/bid guaranty in an amount not less than five percent of the total bid amount when the bidder’s total bid amount as calculated by the Department in accordance with Subsection 103.01 is greater than \$50,000. No proposal/bid guaranty is required for projects when the bidder’s total bid amount as calculated by the Department is \$50,000 or less. The official total bid amount for projects that include alternates is the total of the bidder's base bid and all alternates bid on and accepted by the Department. The proposal/bid guaranty submitted by the bidder shall be a bid bond made payable to the contracting agency as specified on the bid bond form provided in the construction proposal. No other form of security will be accepted.

The bid bond shall be on the "Bid Bond" form provided in the construction proposal, on a form that is materially the same in all respects to the "Bid Bond" form provided, or on an electronic form that has received Department approval prior to submission. The bid bond shall be filled in completely, shall be signed by an authorized officer, owner or partner of the bidding entity, or each entity representing a joint venture; shall be signed by the surety's agent or attorney-in-fact; and shall be accompanied by a notarized document granting general power of attorney to the surety's signer. The bid bond shall not contain any provisions that limit the face amount of the bond.

The bid bond will be written by a surety or insurance company that is in good standing and currently licensed to write surety bonds in the State of Louisiana by the Louisiana Department of Insurance and also conform to the requirements of LSA-R.S. 48:253.

All signatures required on the bid bond may be original, mechanical reproductions, facsimiles or electronic. Electronic bonds issued in conjunction with electronic bids must have written Departmental approval prior to use. The Department will make a listing of approved electronic sureties providers on the Bidx.com site.

SECTION 107 – LEGAL RELATIONS AND RESPONSIBILITY TO PUBLIC:

Subsection 107.05 – Federal Aid Participation (04/08), Pages 57 and 58.

Delete the second paragraph.

SECTION 108 – PROSECUTION AND PROGRESS:

Subsection 108.04 – Prosecution of Work (03/05) Pages 74 and 75.

Add the following sentence to the third paragraph of Heading (b).

Should the surety or the Department take over prosecution of the work, the contractor shall remain disqualified for a period of one year from the completion of the project, unless debarment proceedings are instituted.

When the Department of Transportation and Development is not the contracting agency on the project, the second paragraph under Heading (c) is deleted.

PART II – EARTHWORK

SECTION 202 – REMOVING OR RELOCATING STRUCTURES AND OBSTRUCTIONS:

Subsection 202.06 – Plugging or Relocating Existing Water Wells (03/04), Page 105.

Delete the first sentence and substitute the following.

All abandoned wells shall be plugged and sealed at the locations shown on the plans, or as directed by the engineer, in accordance with the “Water Well Rules, Regulations, and Standards, State of Louisiana.” This document is available at the Department of Transportation and Development, Water Resources Section, P. O. Box 94245, Baton Rouge, Louisiana 70804-9245. The Water Resource Section’s telephone number is (225) 274-4172.

PART III – BASE COURSES

SECTION 302 – CLASS II BASE COURSE:

Subsection 302.05 – Mixing (08/06), Pages 152 and 153.

Delete the first sentence of Subheading (b)(1), In-Place Mixing, and substitute the following.

In-place mixing shall conform to Heading (a)(1) except that the percentage of Type I portland cement required will be 6 percent by volume.

SECTION 305 – SUBGRADE LAYER:

Subsection 305.06 – Payment (01/08), Page 184.

Delete the contents of this subsection and substitute the following.

305.06 Payment. Payment for subgrade layer will be made at the contract unit price which includes lime, lime treatment, cement, cement treatment, water, stone, recycled portland cement concrete, crushed slag, blended calcium sulfate, asphaltic concrete, and asphalt curing membrane or prime coat, subject to the payment adjustment provisions of Section 1002 for specification deviations of asphalt materials and Subsection 303.11(a) for density deficiencies of cement treated materials. Adjustments in pay for increase or decrease in the percent cement ordered by the engineer will be in accordance with Subsection 303.13. Adjustments in pay for

increase or decrease in the percent lime ordered by the engineer will be based on the price of lime shown on paid invoices (total of all charges). The Materials and Testing Section will provide the payment adjustment percentage for properties of asphalt materials.

Payment for geotextile fabric will be included in the contract unit price for subgrade layer.

Payment will be made under:

Item No.	Pay Item	Pay Unit
305-01	Subgrade Layer _____ in (mm) Thick	Square Yard (Sq m)

SECTION 307 – PERMEABLE BASES:

Subsection 307.02 – Materials (09/07), Pages 187 and 188.

Delete the contents of Subheading (b), Asphalt, and substitute the following.

(b) Asphalt: The asphalt for asphalt treated permeable base shall be an approved polymer modified asphalt cement, PG 76-22m, or PG 82-22rm complying with Section 1002. The percentage of asphalt cement shall be 2.0 percent to 4.0 percent by weight (mass) of the total mixture. Asphalt cement content and mixing process shall be such that all aggregates are visibly coated. The mixture shall retain 90 percent coating when tested in accordance with DOTD TR 317.

A job mix formula shall be submitted and approved in accordance with Section 502.

SECTION 308 – IN-PLACE CEMENT TREATED BASE COURSE:

All Subsections within Section 308 – (07/07), Pages 191 – 198.

Whenever the reference to “DOTD TR-432, Method D” is used, it shall mean “DOTD TR-432”.

PART V – ASPHALTIC PAVEMENTS

SECTION 502 – SUPERPAVE ASPHALTIC CONCRETE MIXTURES:

Subsection 502.02 – Materials (08/06) (11/07), Pages 210 – 213.

Delete Table 502-2, Superpave Asphalt Cement Usage under Subheading (a) and substitute the following.

**Table 502-2
Superpave Asphalt Cement Usage**

Current Traffic Load Level	Mixture Type	Grade of Asphalt Cement
Level 1	Wearing Course	PG 70-22m
	Binder Course	PG 70-22m
	Base Course	PG 64-22
Level 2	Wearing Course	PG 76-22m
	Binder Course	PG 76-22m
Level A	Incidental Paving	PG 70-22m

Note: A PG 82-22 m, Waste Tire Rubber Modified Asphalt, may be substituted for any other grade of asphalt cement.

Delete Table 502-3, Aggregate Friction Rating under Subheading (c)(1) and substitute the following.

**Table 502-3
Aggregate Friction Rating**

Friction Rating	Allowable Usage
I	All mixtures
II	All mixtures
III	All mixtures, except travel lane wearing courses with plan ADT greater than 7000 ¹
IV	All mixtures, except travel lane wearing courses ²

¹ When plan current average daily traffic (ADT) is greater than 7000, blending of Friction Rating III aggregates and Friction Rating I and/or II aggregates will be allowed for travel lane wearing courses at the following percentages. At least 30 percent by weight (mass) of the total aggregates shall have a Friction Rating of I, or at least 50 percent by weight (mass) of the total aggregate shall have a Friction Rating of II. The frictional aggregates used to obtain the required percentages shall not have more than 10 percent passing the No. 8 (2.36 mm) sieve.

² When the average daily traffic (ADT) is less than 2500, blending of Friction Rating IV aggregates with Friction Rating I and/or II aggregates will be allowed for travel lane wearing courses at the following percentages. At least 50 percent by weight (mass) of the total aggregate in the mixture shall have a Friction Rating of I or II. The frictional aggregates used to obtain the required percentages shall not have more than 10 percent passing the No. 8 (2.36 mm) sieve.

Subsection 502.14 – Lot Sizes (11/07), Pages 232 and 233.

Delete the first sentence of the first paragraph and substitute the following.

A lot is a segment of continuous production of asphaltic concrete mixture from the same job mix formula produced for the Department at a specific plant, delivered to a specific DOTD project.

SECTION 508 – STONE MATRIX ASPHALT:

Subsection 508.01 – Description (09/07), Page 274.

Delete this subsection and substitute the following.

508.01 DESCRIPTION. This work consists of furnishing and constructing Stone Matrix Asphalt (SMA) which is a plant mixed asphalt concrete wearing course for high traffic applications. This mixture is a rut resistant hot mix design with stone on stone contact. The mixture shall be composed of a PG 76-22m, or PG 82-22rm asphalt cement and a gap graded coarse aggregate structure. Mineral filler and/or fibers shall be used to control draindown. This work shall be in accordance with these specifications, plan details, and as directed. All requirements of Section 502 apply to Stone Matrix Asphalt, except as modified herein. All plant and paving equipment and processes must meet the requirements of Section 503.

Mixture used for shoulder may be Stone Matrix Asphalt or any mixture type shown in Table 502-5.

Subsection 508.02 – Materials (09/07), Page 274.

Delete the contents of subheading (a), Asphalt Cement and substitute the following.

(a) Asphalt Cement: Asphalt cement shall be PG 76-22m, or PG 82-22rm as listed on QPL 41 and complying with Section 1002.

PART VI – RIGID PAVEMENT

SECTION 602 – PORTLAND CEMENT CONCRETE PAVEMENT

REHABILITATION:

Subsection 602.17 – Payment (09/07), Pages 341 – 344.

Delete the last paragraph of Subheadings (d), Full Depth Corner Patching of Jointed Concrete Pavement, (e) Full Depth Patching of Jointed Concrete Pavement, and (g) Patching Continuously Reinforced Concrete Pavement, and substitute the following.

Payment for deteriorated base course removed as directed by the engineer and replaced with concrete will be made as follows: The value per inch (mm) thickness will be determined by dividing the contract unit price per square yard (sq m) by the plan thickness. Thickness of patches will be measured from the surface that exists at the time of patching. Payment for the additional thickness will be made at 50 percent of the value per inch (mm) thus determined.

PART VII – INCIDENTAL CONSTRUCTION

SECTION 701 – CULVERTS AND STORM DRAINS:

All Subsections within Section 701 (08/07), Pages 347 – 358.

Delete Section 701, Culverts and Storm Drains and substitute the following.

SECTION 701
CULVERTS AND STORM DRAINS

701.01 DESCRIPTION. This work consists of furnishing, installing, and cleaning pipe, pipe arch, storm drains and sewers, also referred to as culverts or conduit, in accordance with these specifications and in conformity with lines and grades shown on the plans or established.

701.02 MATERIALS. Materials shall comply with the following sections and subsections:

Usable Soil	203.06(a)
Selected Soil	203.06(b)
Plastic Soil Blanket	203.10
Mortar	702.02
Flowable Fill	710
Portland Cement Concrete	901
Reclaimed Asphaltic Pavement (RAP)	1003.01 & 1003.04(d)
Stone	1003.03(b)
Recycled Portland Cement Concrete	1003.03(c)
Granular Material	1003.07
Bedding Material	1003.08
Concrete Sewer Pipe	1006.02
Reinforced Concrete Pipe	1006.03
Reinforced Concrete Pipe Arch	1006.04
Gasket Materials	1006.06
Plastic Pipe	1006.07
Split Plastic Coupling Bands	1006.07(d)(4)
Plastic Yard Drain Pipe	1006.09
Bituminous Coated Corrugated Steel Pipe and Pipe Arch	1007.02
Structural Plate for Pipe, Pipe Arch and Arch	1007.04
Corrugated Aluminum Pipe and Pipe Arch	1007.05
Coupling Bands	1007.09
Reinforcing Steel	1009
Geotextile Fabric	1019

(a) Side Drain Pipe or Side Drain Pipe Arch: When the item for Side Drain Pipe or Side Drain Pipe Arch is included in the contract, the contractor has the option of furnishing reinforced concrete pipe or reinforced concrete pipe arch, corrugated metal pipe or corrugated metal pipe arch, or plastic pipe, as allowed by EDSM II.2.1.1 or unless otherwise specified.

(b) Cross Drain Pipe or Cross Drain Pipe Arch: When the item for Cross Drain Pipe or Cross Drain Pipe Arch is included in the contract, the contractor has the option of furnishing reinforced concrete pipe or reinforced concrete pipe arch, corrugated metal pipe or corrugated metal pipe arch, or plastic pipe, as allowed by EDSM II.2.1.1 or unless otherwise specified.

(c) Storm Drain Pipe or Storm Drain Pipe Arch: When the item for Storm Drain Pipe or Storm Drain Pipe Arch is included in the contract, the contractor has the option of furnishing reinforced concrete pipe or reinforced concrete pipe arch, or plastic pipe, as allowed by EDSM II.2.1.1 or unless otherwise specified.

(d) Yard Drain Pipe: When the item for Yard Drain Pipe is included in the contract, the contractor has the option of furnishing concrete sewer pipe, plastic yard drain pipe or plastic pipe in accordance with Section 1006 unless otherwise specified.

(e) Material Type Abbreviations:

(1) Reinforced Concrete Pipe:

RCP	Reinforced Concrete Pipe
RCPA	Reinforced Concrete Pipe Arch

(2) Corrugated Metal Pipe:

CAP	Corrugated Aluminum Pipe
CAPA	Corrugated Aluminum Pipe Arch
CMP	Corrugated Metal Pipe
CMPA	Corrugated Metal Pipe Arch
CSP	Corrugated Steel Pipe
CSPA	Corrugated Steel Pipe Arch
BCCSP	Bituminous Coated Corrugated Steel Pipe
BCCSPA	Bituminous Coated Corrugated Steel Pipe Arch

(3) Plastic Pipe:

PP	Plastic Pipe
PVCP	Polyvinyl Chloride Pipe
RPVCP	Ribbed Polyvinyl Chloride Pipe
CPEPDW	Corrugated Polyethylene Pipe Double Wall

(f) Joint Type Abbreviations:

T1	Type 1 Joint
T2	Type 2 Joint
T3	Type 3 Joint

(g) Quality Assurance for Pipe: Manufacturing plants will be periodically inspected for compliance with specified manufacturing methods, and material samples will be randomly obtained for laboratory testing for verification of manufacturing lots. Materials approved at the manufacturing plant will be subject to visual acceptance inspections at the jobsite or point of delivery.

701.03 EXCAVATION. For all pipe, when the sides of the trench are stable as evidenced by the sides of the trench being able to maintain a vertical cut face, the minimum trench width at the bottom of the excavation will be 18 inches (460mm) on either side of the outside diameter of the pipe. If the sides of the trench are unstable, the width of the trench at the bottom of the excavation, for plastic or metal pipe, shall be a minimum width of at least 18 inches (460mm) or one pipe diameter on each side of the outside diameter of the pipe, which ever is greater. Surplus material or excavated material that does not conform to the requirements of Subsection 203.06(a) shall be satisfactorily disposed of in accordance with Subsection 202.02. Moisture controls

including backfill materials selection and dewatering using sumps, wells, well points or other approved processes may be necessary to control excess moisture during excavation, installation of bedding, over-excavated trench backfilling, pipe placement and pipe backfill.

(a) Over-excavation: When unsuitable soils as defined in Subsection 203.04 or a stable, non-yielding foundation cannot be obtained at the established pipe grade, or at the grade established for placement of the bedding, unstable or unsuitable soils below this grade shall be removed and replaced with granular material meeting the requirements of Subsection 1003.07, bedding materials meeting the requirements of Subsection 1003.08 or Type A backfill. All granular, backfill materials placed below the established pipe or bedding grade shall be placed in lifts not exceeding 8 inches (200 mm) thick and sufficiently compacted by hand or a dynamic mechanical hand compaction device over the surface of each lift to form a stable, non-yielding foundation at the surface of the established bedding or pipe grade.

When rock is encountered, it shall be removed below grade and replaced with material complying with Subsection 1003.07, bedding materials meeting the requirements of Subsection 1003.08 or Type A backfill. The compacted earth cushion shall have a thickness under the pipe of at least 1/2 inch per foot (40 mm/m) of fill height over the top of the pipe with a minimum thickness of 8 inches (200 mm). All granular, backfill materials placed below the established pipe or bedding grade shall be placed in lifts not exceeding 8 inches (200 mm) thick and sufficiently compacted by hand or a dynamic mechanical hand operated compaction device over the surface of each lift to form a stable, non-yielding foundation at the surface of the established bedding or pipe grade.

Materials used to backfill in an over-excavated portion of a trench do not require encasement in a Geotextile Fabric.

Density of approved materials placed in over-excavated trenches will not be measured or determined.

701.04 FORMING PIPE BED. Bedding material, when specified, shall be constructed in accordance with Section 726. Materials allowed for bedding shall be as specified in Subsection 1003.08 or may be Type A backfill materials. When bedding materials are specified, additional excavation shall be performed below established pipe grade and the bedding material placed in lifts not exceeding 8 inches (200 mm) thick and lightly compacted by hand or a dynamic hand compaction device over the surface of each lift.

When the bottom of the pipe is not laid in a trench but is constructed above natural soils, a uniform bed shall be constructed as specified for the bottom of a trench.

Density of approved bedding materials will not be measured or determined.

701.05 LAYING PIPE. Pipe laying shall begin at the downstream end of the line. The pipe shall be in contact with the foundation throughout its length. Bell or groove ends of pipe and outside circumferential laps of riveted metal pipe shall be placed facing upstream. Riveted seam metal pipe shall be placed with longitudinal laps at sides. Pipes in each continuous line shall have the same wall thickness. Metal pipes provided with lifting lugs shall be handled only by these lugs.

After pipe has been laid and before backfill is placed, the engineer will inspect the pipe for alignment, grade, integrity of joints, and coating damage.

701.06 JOINING PIPE.

(a) Joint Usage:

(1) Type 1 (T1) joints shall be used for side drains under drives and similar installations.

(2) Type 2 (T2) joints shall be used for cross drains under roadways, including turnouts.

(3) Type 3 (T3) joints shall be used for closed storm drain systems, flumes and siphons.

(b) Concrete Pipe: Concrete pipe may be either bell and spigot, or tongue and groove. The method of joining pipe sections shall be such that ends are fully entered and inner surfaces are flush and even.

An approved mechanical pipe puller shall be used for joining pipes over 36 inches (900 mm) in diameter. For pipe 36 inches (900 mm) or less in diameter, any approved method for joining pipe may be used which does not damage the pipe.

Joints shall comply with Subsection 1006.05, and shall be sealed with gasket material installed in accordance with the manufacturer's recommendations.

(c) Metal Pipe: Metal pipe shall be firmly joined by coupling bands. Bands shall be centered over the joint.

For Type 1 joints, approved gasket material shall be placed in one corrugation recess on each side of the joint at the coupling band and on each band connection in such manner to prevent leakage.

When Type 2 or 3 joints are specified, joining of metal pipe sections shall conform to the following provisions:

(1) General: Band joints shall be sealed with gasket material. Gasket material shall be placed in accordance with the plan details.

(2) Circular Section: Connecting bands shall be of an approved design and shall be installed in accordance with plan details.

(3) Arch Section: Connecting bands shall be a minimum of 12 inches (300 mm) wide for pipe arch less than 36 inches (900 mm) round equivalent diameter, and a minimum of 21 inches (525 mm) wide for 36 inches (900 mm) round equivalent diameter pipe arch and greater. Bands shall be connected at the ends by approved angle or strap connections. Connecting bands used for 36 inches (900 mm) round equivalent diameter pipe arch and above shall be 2-piece bands.

(d) Plastic Pipe: Joints for plastic pipe shall be either bell and spigot or split coupling bands.

(1) Bell and Spigot Type Joint System: The method of joining pipe sections shall be such that ends are fully entered and inner surfaces are flush and even.

Any approved method for joining pipe may be used which does not damage the pipe.

Joints shall be approved and shall be sealed with a gasket system utilizing gasket material complying with Subsection 1006.06(a).

(2) Split Coupling Type Joint System: Split coupling bands shall comply with all dimensional and material requirements of Subsection 1006.07. The bands shall be centered over the joint. The split coupling band shall be secured to the pipe with a minimum of five stainless steel or other approved corrosion resistant bands.

Joints shall be approved and shall be sealed with gasket material. Gasket material shall be placed in the first two corrugation recesses on each side of the pipe connections. Gasket material shall also be placed on each band connection to prevent leakage. When flexible plastic gasket material is used it shall be a minimum of 1/2 inch (13 mm) in size. The bands shall be tightened to create overlap of the band and shall adequately compress the gasket material.

(e) Connections: Approved connections shall be used when joining new pipes to existing pipes. When concrete collars are required in order to extend the ends of existing pipes that have been damaged or to join different types or sizes of pipes, the concrete collars shall be constructed in accordance with plan details, the applicable requirements of Section 901, and as directed.

(f) Geotextile Fabric, Pipe Joints: For concrete, metal and plastic pipes, Types 2 and 3 joints shall be wrapped with geotextile fabric for a minimum of 12 inches (300 mm) on each side of joint for pipe 36 inches (900 mm) or less in diameter and a minimum of 18 inches (450 mm) on each side of the joint for pipe greater than 36 inches (900 mm) in diameter. Ends of the fabric shall be lapped at least 10 inches (250 mm). The edges and ends of fabric shall be suitably secured for the entire circumference of the pipe.

701.07 RELAYING PIPE. If specified or directed, existing pipes shall be removed and suitable sections relaid as specified for new pipes.

701.08 BACKFILLING.

(a) General: Prior to backfilling, pipes found to be damaged or out of alignment or grade shall be removed and reinstalled, or replaced.

Type A backfill material shall be stone, recycled portland cement concrete, flowable fill, or RAP.

Type B backfill materials are selected soils. Where Type B backfill materials are called for, Type A backfill materials may be substituted.

When corrugated metal pipe is used, the backfill material shall be tested and shall have a resistivity greater than 1500 ohm-cm and a pH greater than 5 when tested in accordance with DOTD TR 429 and DOTD TR 430 respectively.

When Type A backfill material is used, geotextile fabric surrounding this backfill shall be placed in accordance with Subsection 726.03 between the aggregate backfill material and all other natural or placed soils in the trench or embankment. Care shall be taken to prevent damage to geotextile fabric during placement of backfill material. For concrete pipe, the fabric shall enclose not only the initial backfill but shall be wrapped over the top of the pipe with at least 12 inches (300 mm) of overlap.

When a trench box or trench sheeting is used in unstable soils and/or for worker safety, and when moved during backfilling operations, filling and additional compaction of the disturbed zone of backfill must take place immediately and in a manner acceptable to the engineer.

Initial backfill is a structural backfill encasing the pipe from the bottom of the pipe to the springline for concrete pipe and to a point one foot (0.3 m) above the top of the pipe for both metal and plastic pipe. Final backfill is not a structural backfill and shall extend from the top of the initial backfill to the top of the natural ground or subgrade in cut areas or to the top of existing ground in fill areas. Any fill required above the final backfill is considered and treated as embankment.

(b) Backfill Applications: For projects using A+B+C bidding method where rigid and flexible pavement alternates are considered, backfill application (2) below, "Cross Drains Under Flexible Pavements", shall apply for either rigid or flexible pavements.

(1) Under Concrete Pavements: Type B backfill may be used as initial and final backfill for all pipes, culverts or drains under concrete pavements. Placement and compaction shall be as specified in Heading (d) below.

(2) Cross Drains Under Flexible Pavements: All reaches, exclusive of those portions of the pipe which are under shoulders, of cross drains and all other culverts, pipes or drains that cross the centerlines of the new roadway or centerlines of existing roadways, such as intersections and are under flexible pavements shall receive an initial backfill of Type A material. Type B backfill materials may be used as final backfill for all pipes. Placement and compaction shall be as specified in Heading (c) and (d) below. Where the subgrade is above existing ground, embankment material as specified for the remainder of the project shall be used from the top of the final backfill to the top of the established embankment grade.

(3) Other Drains Under Flexible Pavements: All reaches of all culverts, pipes or drains under flexible pavements that do not cross the centerlines of new roadway or centerlines of existing roadways, and exclusive of those portions of the pipe which are totally under shoulders, shall receive an initial and final backfill of Type B material. Placement and compaction shall be as specified in Heading (d) below. Where the subgrade is above existing ground, embankment material as specified for the remainder of the project shall be used from the top of the final backfill to the top of the established embankment grade.

(4) Other Areas: All culverts, pipes or drains in nonpaved areas or paved areas that serve as driveways or shoulders shall receive an initial and final backfill of Type B material. Placement and compaction shall be as specified in Heading (d) below.

(5) Pipes Subject to Construction Traffic; The embankment or pipe backfill shall be constructed to a minimum of 24 inches (600 mm) over the pipe before heavy construction equipment is allowed to cross the installation. Where practical, installations with less than 24 inches (600 mm) of cover over the top of the pipe shall be constructed after heavy hauling is completed over the pipe location. After completion of hauling operations, the contractor shall remove excess cover material. Pipe damaged by hauling and backfilling operations shall be removed and reinstalled, or replaced, at no direct pay.

(c) Placement and Compaction; Type A Backfill: For all pipes, culverts and conduits under paved and nonpaved areas, where Type A backfill material is used, the Type A backfill shall be thoroughly hand compacted under the pipe haunches and then dynamically compacted in layers not exceeding 8 inches (200 mm) compacted thickness. Compaction under the haunches of the pipe shall initially be by hand tamping or other acceptable means, until a level is reached that the dynamic tamping can commence. Each lift shall be compacted by applying at least eight

passes of a hand operated, dynamic mechanical compaction device over the surface of each lift. With approval of the engineer, layer thickness may be increased to 12 inches (300 mm) with verification of satisfactory installation and performance. If flowable fill is used it shall be furnished, placed and consolidated in accordance with Section 710. The contractor shall control placement operations during initial backfill operations so as not to damage protective coatings on metal pipes. The contractor shall repair damaged coatings at no additional pay.

(d) Placement and Compaction; Type B Backfill: For all pipes, culverts and conduits, where Type B backfill is allowed, the Type B material shall be placed in layers not exceeding 8 inches (200 mm) compacted thickness. Compaction shall be with suitable mechanical equipment. With approval of the engineer, layer thickness may be increased to 12 inches (300 mm) with verification of satisfactory installation and performance.

(e) Placement and Compaction; Trenchless or Partial Trench Condition: All pipes, culverts, drains and conduits placed with any portion of the pipe above existing ground must also comply with Subsections (a),(b) (c) and (d) above for the portion of the pipe within a trench and that portion of the pipe not constructed in a trench. The width of initial and final backfill of that portion above existing ground and not within a trench will be constructed to such a width that the requirements for placement, compaction and density are met.

(f) Density Requirements: The in place density of Type A backfill materials and bedding materials, will not be measured or determined. Type A backfill, exclusive of RAP and flowable fill, shall be placed at or near optimum moisture content determined in accordance with DOTD TR 415 or 418. RAP materials shall be placed and compacted in a slightly moist condition.

The maximum dry density of initial or final Type B backfill under all paved areas which are to be under traffic will be determined in accordance with DOTD TR 415 or TR 418 and in-place density determined in accordance with DOTD TR 401. Initial and final Type B backfill under all paved areas, under traffic, shall be placed at or near optimum moisture content determined in accordance with DOTD TR 415 or TR 418. Each layer shall be compacted by approved methods prior to the placement of a subsequent layer. The engineer will approve the compaction method based upon validation that such method, including moisture control, will achieve at least 95 percent of maximum dry density as determined in accordance with DOTD TR 401. With approval of the engineer, density testing may be waived on subsequent layers with backfill installation in accordance with approved compaction methods and continued satisfactory performance.

Initial and final backfill in unpaved areas or paved areas such as shoulders or driveways, shall be placed evenly and compacted along the length of the culvert, pipe or drain from the top of the initial backfill to the top of the subgrade. Layered backfill shall be compacted at least to the density of the adjoining existing soils or the compaction required of the laterally adjoining layers of soil immediately outside the trench for embankment elevations. Initial and final backfill shall be placed and compacted at or near optimum moisture content determined in accordance with DOTD TR 415 or TR 418.

701.09 INSPECTION OF PIPES. After completion of embankment and prior to roadway surfacing, the engineer shall inspect pipes for proper alignment and integrity of joints. Any misaligned pipe or defective joints shall be corrected by the contractor at no direct pay.

(a) Plastic Pipe: Installed plastic pipe shall be tested to ensure that vertical deflections do not exceed 5.0 percent. Maximum allowable deflections shall be governed by the mandrel requirements stated herein.

Deflection tests shall be performed no sooner than 30 calendar days after installation and compaction of backfill. The pipe shall be cleaned and inspected for offsets and obstructions prior to testing.

For pipe 36 inches (900 mm) and less in diameter, a mandrel shall be pulled through the pipe by hand to ensure that maximum allowable deflections have not been exceeded. The mandrel shall be approved by the engineer prior to use. Use of an unapproved mandrel or a mandrel altered or modified after approval will invalidate the test. If the mandrel fails to pass, the pipe is overdeflected.

Unless otherwise permitted, overdeflected pipe shall be uncovered and, if not damaged, reinstalled. Damaged pipe shall not be reinstalled, but shall be removed and replaced with new pipe. Any pipe subjected to any method or process other than removal, which attempts, even successfully, to reduce or cure any overdeflection, shall be removed and replaced with new pipe.

The mandrel shall be a rigid, nonadjustable, odd-numbered legged (minimum 9 legs) mandrel having a length not less than its nominal diameter or 24 inches (600 mm), whichever is less. The minimum diameter at any point shall be 5.0 percent less than the base inside diameter of the pipe being tested. The mandrel shall be fabricated of steel, aluminum or other approved material fitted with pulling rings at each end. The nominal pipe size and outside diameter of the mandrel shall be stamped or engraved on some segment other than a runner. A suitable carrying case shall be furnished.

For pipe larger than 36 inches (900 mm) in diameter, deflection shall be determined by a method approved by the engineer. If a mandrel is selected, the minimum diameter, length, and other requirements shall conform to the above requirements.

Mandrel testing shall be conducted by the contractor in the presence of the engineer. Mandrel testing shall be at no direct pay.

(b) Metal Pipe: If the inside diameter of metal pipe or rise dimension of metal pipe arch deflects more than 5.0 percent from original dimensions, they shall be removed and reinstalled, unless they do not rebound or are damaged. Pipe or pipe arch which are damaged or do not rebound shall be removed and replaced at no direct pay. Measurement of deflection will be made by the engineer away from rerolled ends.

701.10 CLEANING PIPES.

(a) Existing Pipes: Pipes designated to be cleaned shall be cleaned of soil, debris and other materials to the invert of the pipe. Designated pipes shall be cleaned by approved methods that will not damage the pipes. Any damage caused by the contractor's operations shall be satisfactorily repaired at no direct pay.

Removed soil, debris and other materials shall be disposed of in accordance with Subsection 202.02 or as otherwise approved in writing.

(b) Contractor Installed Pipes: Prior to final acceptance, pipes shall be cleaned of all debris and soil to the invert of the pipe at no direct pay.

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Removed soil, debris and other materials shall be disposed of in accordance with Subsection 202.02 or as otherwise approved in writing.

701.11 STUBBING AND PLUGGING PIPES. When it is required that pipes be plugged, such plugs shall be constructed of Class R concrete complying with Section 901. Thickness of plug and method of construction shall be as directed.

When new pipes are to be stubbed into new or existing pipes or other structures, the connection shall be made with approved mortar complying with Subsection 702.02.

701.12 MEASUREMENT. Pipe, both new and relaid, will be measured in linear feet (lin m) as follows unless stated otherwise.

(a) Pipe not confined by fixed structures will be measured by the number of joints at the nominal length of each joint.

(b) Pipe confined by fixed structures will be measured along the pipe between the termini of pipe in structure walls.

(c) Pipe confined by a fixed structure on one end and unconfined at the other end will be measured along the pipe from the terminus of pipe in the structure wall to the unconfined end of pipe.

(d) Fabricating of pipe tees, elbows and other fittings will be measured per each fitting. The length of pipe in such fittings will be included in the pay length measurement of pipes of which they form a part.

(e) Excavation required for installation of pipes will not be measured for payment, except as otherwise specified in Subsection 203.14.

(f) Furnishing and placing backfill material below existing ground level for pipes will not be measured for payment. Backfill material needed to complete backfill above natural ground and around pipes that extend above natural ground will be measured and payment will be made under applicable earthwork items. When specified, flowable fill will be measured and paid for in accordance with Section 710.

(g) Plugging and stubbing of pipes will not be measured for payment.

(h) Cleaning existing pipes will be measured by the length of pipe cleaned and accepted.

(i) Concrete collars will be measured per each.

701.13 PAYMENT.

(a) Payment for pipe will be made at the contract unit price per linear foot (lin m) of the types and sizes specified.

When plastic pipe is specified on the plans or elected to be used by the contractor, payment will be made at the contract unit price per linear foot (lin m) of the types and sizes specified in accordance with the payment schedule of Table 701-1.

Table 701-1
Payment Schedule for Plastic Pipe

Percent Payment	Stage of Completeness
75	After placement and backfill has been completed
25	After the pipe has met vertical deflection requirements in accordance with Subsection 701.09(a)

(b) Payment for fabricating pipe tees, elbows and other fittings will be made at the contract unit price per each fitting.

(c) When unstable conditions are encountered, the additional excavation will not be measured for payment; however, the additional materials furnished and placed for the pipe foundation will be measured and paid for as follows:

(1) Granular Materials: Payment will be made under the embankment item. The net section volume of the materials will be multiplied by 3 to determine the pay volume. When the contract does not include a pay item for embankment, payment will be made in accordance with Subsection 104.02.

(2) Bedding Material: Measurement and payment will be made in accordance with Section 726. When the contract does not include a pay item for bedding material, payment will be made in accordance with Subsection 104.02.

(d) Payment for cleaning existing pipes will be made at the contract unit price per linear foot (lin m).

(e) Payment for concrete collars will be made at the contract unit price per each.

Payment will be made under:

Item No.	Pay Item	Pay Unit
701-01	Cross Drain Pipe (Size & Type)	Linear Foot (Lin m)
701-02	Cross Drain Pipe Arch (Size & Type)	Linear Foot (Lin m)
701-03	Storm Drain Pipe (Size & Type)	Linear Foot (Lin m)
701-04	Storm Drain Pipe Arch (Size & Type)	Linear Foot (Lin m)
701-05	Side Drain Pipe (Size)	Linear Foot (Lin m)
701-06	Side Drain Pipe Arch (Size)	Linear Foot (Lin m)
701-07	Yard Drain Pipe (Size)	Linear Foot (Lin m)
701-08	Relaying Pipe	Linear Foot (Lin m)
701-09	Fabricating Pipe Fittings	Each
701-10	Reinforced Concrete Pipe (Extension)	Linear Foot (Lin m)
701-11	Reinforced Concrete Pipe Arch (Extension)	Linear Foot (Lin m)
701-12	Corrugated Metal Pipe (Extension)	Linear Foot (Lin m)
701-13	Corrugated Metal Pipe Arch (Extension)	Linear Foot (Lin m)

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701-14	Cleaning Existing Pipes	Linear Foot (Lin m)
701-15	Concrete Collar	Each
701-16	Plastic Pipe (Extension)	Linear Foot (Lin m)

SECTION 704 – GUARD RAIL:

Subsection 704.03 – General Construction Requirements (01/05), Pages 368 and 369.

Add the following to Heading (d), Guard Rail End Treatments.

All end treatments shall bear a label indicating the manufacturer and exact product name of the end treatment along with its assigned NCHRP 350 test level. This label shall resist weathering and shall be permanently affixed to the railing in such a way as to be readily visible.

SECTION 706 – CONCRETE WALKS, DRIVES AND INCIDENTAL PAVING:

All Subsections within Section 706 (04/08), Pages 375 – 377.

Delete Section 706, Concrete Walks, Drives and Incidental Paving and substitute the following.

**SECTION 706
CONCRETE WALKS, DRIVES AND INCIDENTAL PAVING**

706.01 DESCRIPTION. This work consists of furnishing and constructing portland cement concrete walks, handicapped curb ramps, drives and incidental paving slabs in accordance with these specifications and in conformity with lines, grades and dimensions shown on the plans or established.

706.02 MATERIALS. Materials shall comply with the following Section or Subsections.

Portland Cement Concrete (Class M)	901
Joint Filler	1005.01(c)
Reinforcing Steel	1009.01
Curing Materials	1011.01

706.03 CONSTRUCTION REQUIREMENTS.

(a) Excavation: Excavation shall be made to required depth and width. The top of the subgrade shall be shaped and compacted to a firm, even surface conforming to the section shown on the plans. Unsuitable material shall be removed and disposed of in accordance with Subsection 202.02 and replaced with approved material at no direct pay.

(b) Forms: Forms shall be of wood or metal and shall extend the full depth of concrete. Forms shall be straight, clean and of sufficient strength to resist the pressure of concrete. Bracing of forms shall be such that forms remain in horizontal and vertical alignment until their removal.

Concrete may be placed by slip-form methods. Slip-formed concrete shall be placed with an approved machine designed to spread, vibrate, consolidate and finish concrete in one pass of the machine in such manner that minimum hand finishing is necessary. Sliding forms shall be

rigidly held together to prevent spreading of forms. After the passing of the side forms there shall be no noticeable slumping of concrete.

(c) Subgrade: The subgrade shall be thoroughly moistened immediately prior to placing concrete.

(d) Placing and Finishing: Concrete shall be placed on the subgrade, struck off to required thickness and tamped sufficiently to bring the mortar to the surface. The surface shall be finished with a wood float or steel trowel followed by brushing to a slightly rough finish. Joints and edges shall be rounded with an edging tool having a 1/4-inch (6 mm) radius.

(e) Joints:

(1) Expansion Joints: Expansion joints shall be filled with 1/2 inch (13 mm) thick preformed expansion joint filler. Expansion joints shall be installed at maximum 100-foot (30 m) intervals, and between intersecting paving and any fixed structure such as a building, bridge or curbing, and between intersecting paving and the handicapped curb ramps. Expansion joint material shall extend for the full width and depth of paving.

(2) Weakened Plane: Weakened planes shall be formed by a jointing tool or other acceptable means. Weakened planes shall extend into concrete for at least 1/4 of the depth and shall be approximately 1/8 inch (3 mm) wide.

a. Walks: Spacing of weakened planes for walks shall be equal to the width of walk.

b. Drives: A longitudinal weakened plane shall be formed along the centerline of drives more than 16 feet (5 m) wide, and transverse weakened planes shall be formed at not more than 16-foot (5 m) intervals.

c. Incidental Paving: Weakened planes for incidental paving shall be formed at intervals not exceeding 30 times the thickness of the concrete in length or width. Incidental paving poured adjacent to jointed concrete shall be jointed to match existing joints, with intermediate joints formed as necessary not to exceed the maximum joint spacing.

(3) Construction Joints: Construction joints shall be formed around manholes, utility poles, etc., extending into paving and 1/4 inch (6 mm) thick preformed expansion joint filler shall be installed in these joints.

(4) Tie-ins: Tie-ins of existing concrete shall be made by full depth sawing at no direct pay.

(f) Curing: Concrete shall be cured in accordance with Subsection 601.10.

(g) Detectable Warning Surface for Handicap Ramps and At-Grade Sidewalk Intersections: Sidewalks, when intersecting with roadways, shall be equipped with a detectable warning surface system consisting of raised truncated domes as a transition between the sidewalk and the street as required by the Americans with Disabilities Act, 28 CFR Part 36, ADA Standards for Accessible Design.

Detectable warnings (truncated domes) shall be installed on the ramp surface over the full width of the ramp throat for a distance of 24 inches (600 mm) in the direction of travel from the back of the curb. Detectable warnings (truncated domes) shall also be installed on at-grade sidewalks intersecting with roadways for a distance of 36 inches (900 mm) in the direction of travel from the end of the sidewalk. Truncated domes shall be laid out on a square grid in order to allow enough space for wheelchairs to roll between the domes.

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Light reflectance of the truncated domes and the underlying surface must meet the 70 percent contrast requirement of ADAAG.

706.04 MEASUREMENT. Quantities of concrete walks, drives and incidental paving slabs for payment will be the design quantities as specified on the plans and adjustments thereto. Design quantities will be adjusted if the engineer makes changes to adjust to field conditions, if design errors are proven or if design changes are made. Design areas are based on the horizontal dimensions shown on the plans. Excavation, backfill, reinforcing steel and joint materials will not be measured for payment.

Handicapped curb ramps, including the detectable surface warning system, will be measured per each.

Detectable surface warning systems for at-grade sidewalk intersection will not be measured for payment.

706.05 PAYMENT. Payment for concrete walks, drives and incidental paving will be made on a lot basis at the contract unit price per square yard (sq m), adjusted in accordance with the following provisions. Payment for each lot will be made in accordance with Table 901-6. Size, sampling, and testing of each concrete lot shall be in accordance with the Materials Sampling Manual.

Payment for handicapped curb ramps, including the detectable surface warning system, will be made by each and shall include, but not limited to, curb transitions, detectable warning system, gutter, landing and base.

Payment will be made under:

Item No.	Pay Item	Pay Unit
706-01	Concrete Walk (inch (mm) Thick)	Square Yard (Sq m)
706-02	Concrete Drive (inch (mm) Thick)	Square Yard (Sq m)
706-03	Incidental Concrete Paving (inch (mm) Thick)	Square Yard (Sq m)
706-04	Handicapped Curb Ramps	Each

SECTION 713 – TEMPORARY TRAFFIC CONTROL:

Subsection 713.06 – Pavement Markings (08/06), Pages 400 – 403.

Delete Table 713-1, Temporary Pavement Markings and substitute the following.

Table 713-1
Temporary Pavement Markings^{1,2}

		Two-lane Highways	Undivided Multilane Highways	Divided Multilane Highways
S H O R T T E R M	ADT<1500; or ADT>1500 and time<3 days	Lane lines 4-foot (1.2 m) tape on 40-foot (12 m) centers; with "Do Not Pass" and "Pass With Care" signs as required		
	ADT>1500; Time>3 days and<2 weeks	Lane lines 4-foot (1.2-m) tape on 40-foot (12-m) centers with no passing zone markings		
	All ADT's with time <2 weeks		Lane lines 4-foot (1.2m) tape on 40-foot (12 m) centers; double yellow centerline	Lane lines 4-foot (1.2 m) tape on 40-foot (12 m) centers
L O N G T E R M	All ADT's with time >2 weeks	Standard lane lines, no-passing zone markings, legends and symbols and when pavement width is 22 feet (6.7 m) or greater, edge lines	Standard lane lines, centerlines, edge lines, and legends and symbols	Standard lane lines, centerlines, edge lines, and legends and symbols.

¹No-passing zones shall be delineated as indicated whenever a project is open to traffic.

²On all Asphaltic Surface Treatments that are open to traffic and used as a final wearing course or as an interlayer, temporary pavement markings (tabs) on 20-foot (6 m) centers shall be used, in lieu of the 4-foot (1.2 m) tape, on 40-foot (12 m) centers.

SECTION 729 – TRAFFIC SIGNS AND DEVICES:

Subsection 729.02 – Materials (04/08), Pages 456 and 457.

Delete the contents of Heading (a), Sign and Marker Sheeting, and substitute the following.

(a) Sign and Marker Sheeting: Sheeting material for sign panels, delineators, barricades and other markers shall comply with Section 1015. All permanent signs shall meet the requirements of ASTM D 4956, Type X.

Subsection 729.04, Fabrication of Sign Panels and Markers (04/08), Pages 458 – 460.

Delete the third paragraph of Heading (c), Sheeting Application and substitute the following.

ASTM D 4956 Type X reflective sheeting shall be applied with an orientation determined by the engineer to obtain the optimum entrance angle performance. Fabricated vertical splices in ASTM D 4956 Type X reflective sheeting will be allowed only when the horizontal dimension of the sign face or attached shield is in excess of the maximum manufactured width of the sheeting. Fabricated vertical splices in ASTM D 4956 Type X reflective sheeting will also be allowed when the specified orientation will create excessive sheeting waste.

SECTION 804 – DRIVEN PILES:

Subsection 804.08 – Construction Requirements (04/07), Pages 548 – 554.

Delete the first sentence of Heading (a), Preboring and substitute the following.

Preboring by augering, wet-rotary drilling, or other methods used to facilitate pile driving will not be permitted unless specified in the plans or allowed by the engineer.

Delete the first sentence of Heading (b), Jetting and substitute the following.

Jetting will not be permitted unless allowed in the plans or allowed by the engineer.

SECTION 901 – PORTLAND CEMENT CONCRETE:

Subsection 901.06 – Quality Control of Concrete (08/06), Pages 726 – 731.

Add the following to the contents of Heading (b), Quality Control Tests.

The contractor shall be responsible for monitoring the components (cement, mineral and chemical admixtures, aggregates) in their mix to protect against any changes due to component variations. As component shipments arrive, the contractor shall verify slump, air content and set time by testing at ambient temperatures. The contractor shall make adjustments to the mix design to rectify any changes which would adversely affect constructability, concrete placement or the specifications. The contractor shall submit test results to the Department for review each day of paving. Testing to validate component consistency will be documented on the control logs. Conformance or variation in mix parameters (workability, set times, air content, etc.) shall be noted on the control logs. The contractor shall provide a copy of the proposed testing plan to the engineer for record. Acceptance of the plan does not relieve the contractor's responsibility for consistency.

Subsection 901.08 – Composition of Concrete (12/05), Pages 732 – 734.

Add the following to Heading (a).

The blended cement containing up to 50 percent of grade 100 or grade 120 ground granulated blast-furnace slag must be in compliance with Subsection 1001.04 for portland blast-furnace slag cement.

SECTION 1001 – HYDRAULIC CEMENT:

Subsection 1001.01 – Portland Cement (09/07). Page 749.

Delete the contents of this subsection and substitute the following.

1001.01 PORTLAND CEMENT. Portland cement shall be from an approved source listed in QPL 7 and shall comply with AASHTO M 85.

Alkali content calculated as sodium oxide equivalent shall not exceed 0.60 percent by weight for all types of cement.

SECTION 1003 – AGGREGATES:

Subsection 1003.02 – Aggregates for Portland Cement Concrete and Mortar (07/07),

Pages 763 – 766.

Delete the contents of Heading (c), Aggregates for Types B and D Pavements, and substitute the following.

(c) Aggregates for Types B and D Pavements: For the combined aggregates for the proposed portland cement concrete pavement mix, the percent retained based on the dry weight (mass) of the total aggregates shall meet the requirements of Table 1003-1A for the type of pavement specified in the plans. Additionally, the sum of the percents retained on any two adjacent sieves so designated in the table shall be at least 12 percent of the total combined aggregates. The maximum amounts by weight (mass) of deleterious materials for the total aggregate shall be the same as shown in Subsection 1003.02(b).

Table 1003-1A
Aggregates for Types B and D Pavements

U.S. Sieve	Metric Sieve	Percent Retained of Total Combined Aggregates	
		Pavement Type	
		Type B	Type D
2 1/2 inch	63 mm	0	0
2 inch	50 mm	0	0-20
1 1/2 inch	37.5 mm	0-20	0-20
1 inch	25.0 mm	0-20	5-20
3/4 inch	19.0 mm	5-20	5-20
1/2 inch	12.5 mm	5-20	5-20
3/8 inch	9.5 mm	5-20	5-20
No. 4	4.75 mm	5-20	5-20
No. 8	2.36 mm	5-20	5-20
No. 16	1.18 mm	5-20	5-20
No. 30	600 µm	5-20	5-20
No. 50	300 µm	0-20	0-20
No. 100	150 µm	0-20	0-20
No. 200	75 µm	0-5	0-5

Note: For the sieves in the shaded areas, the sum of any two adjacent sieves shall be a minimum of 12 percent of the total combined aggregates.

Each type of aggregate to be used in the proposed mixture shall be sampled and tested individually. The percent of total combined aggregates retained shall be determined mathematically based on the proportions of the combined aggregate blend. All gradation calculations shall be based on percent of dry weight (mass).

SECTION 1005 – JOINT MATERIALS FOR PAVEMENTS AND STRUCTURES:

Subsection 1005.04 – Combination Joint Former/Sealer (11/05), Pages 782 and 783.

Delete Heading (a) and substitute the following.

(a) Description: This joint former/sealer is intended for use in simultaneously forming and sealing a weakened plane in portland cement concrete pavements.

The material shall consist of an elastomeric strip permanently bonded either mechanically or chemically at the top of each of two rigid plastic side frames and covered with a removable plastic top cap. Side frames shall be of such configuration that when the sealer is inserted into plastic concrete and vibrated, a permanent bond forms between side frames and concrete.

Delete Heading (b)(1) and substitute the following.

(1) Elastomer: The elastomer strip portion of the material shall be manufactured from vulcanized elastomeric compound using polymerized chloroprene or thermoplastic vulcanizate as the base polymer, and shall comply with the following requirements:

<u>Property</u>	<u>ASTM Test Method</u>	<u>Requirements</u>	
		<u>Polymerized Chloroprene</u>	<u>Thermoplastic Vulcanizate</u>
Tensile Strength, kPa, Min.	D 412	12,400	7,400
Elongation at Break, % Min.	D 412	200	400
Hardness, Shore A	D 2240	65 ± 10	65 ± 10
Properties after Aging, 70 h @ 100°C	D 573		
Tensile Strength, % Loss, Max.		20	20
Elongation, % loss, Max.		25	25
Hardness, pts. increase, Max.		10	10
Ozone Resistance, 20% strain or bentloop, 300 pphm in air, 70 h @ 40°C	D 1149	no cracks	no cracks
Oil Swell, IRM 903, 70 h @ 100°C, wt change, % Max.	D 471	45	75

Delete Headings (b)(2) and (b)(3) and substitute the following:

(2) Bond of Elastomer to Plastic: The force required to shear the elastomer from the plastic shall be a minimum of 5.0 pounds per linear inch (90 g/mm) of sealer when tested in accordance with DOTD TR 636.

(3) Bond of Plastic to Cement Mortar: This bond will be evaluated and shall meet the following requirements:

The force required to separate the cement mortar from the plastic shall be a minimum of 5.0 pounds per linear inch (90 g/mm) of sealer when tested in accordance with DOTD TR 636.

SECTION 1006 – CONCRETE AND PLASTIC PIPE:

Subsection 1006.09 – Plastic Yard Drain Pipe (06/07), Page 789.

Delete the contents of Subheading (a)(3), Ribbed Polyvinyl Chloride Pipe (RPVCP) and substitute the following.

Ribbed Polyvinyl Chloride Pipe (RPVCP): Ribbed Polyvinyl Chloride Pipe shall comply with ASTM F 794, Series 46 or ASTM F 949 (46 psi).

SECTION 1013 – METALS:

Subsection 1013.09 – Steel Piles (08/06) Page 822.

Delete the title and references to “Steel Piles” in this subsection and substitute “Steel H Piles”.

SECTION 1015 – SIGNS AND PAVEMENT MARKINGS:

Subsection 1015.04 – Sign Panels (05/07), Pages 832 and 833.

Delete the contents of Heading (a), Permanent Sign Panels and substitute the following.

Supplemental Specifications (August 2008)

Page 24 of 30

(a) Permanent Sign Panels: Flat panels shall be aluminum sheets or plates complying with ASTM B 209, Alloy 6061-T6 or Alloy 5052-H38. Extruded aluminum panels shall comply with ASTM B 221 (ASTM B 221M), Alloy 6063-T6 and after fabrication, have a flatness equal to or less than 0.031 inch per foot of length and 0.004 inch per inch of width.

Subsection 1015.05 - Reflective Sheeting (04/08), Pages 833 – 838.

Delete the contents of this subsection and substitute the following.

1015.05 REFLECTIVE SHEETING.

(a) Permanent and Temporary Standard Sheeting: Reflective sheeting shall be one of the following standard types as specified on the plans and complying with ASTM D 4956 except as modified herein. Permanent warning, regulatory, guide and supplemental guide sign sheeting shall meet the requirements of ASTM D 4956 Type X. Reflective sheeting for temporary signs and devices shall meet the requirements of ASTM D 4956 Type III except as noted in Subsection 1015.05(f). Reflective sheeting shall be an approved product listed in QPL 13.

Type III - A high-intensity retroreflective sheeting that is typically encapsulated glass-bead retroreflective material.

Type VI - An elastomeric high-intensity retroreflective sheeting without adhesive. This sheeting is typically a vinyl microprismatic retroreflective material.

Type X - A super high-intensity retroreflective sheeting having highest retroreflectivity characteristics at medium distances. This sheeting is typically an unmetalized microprismatic retroreflective element material.

(b) Fluorescent Pink Retroreflective Sheeting: Signs for temporary control of traffic through incident management areas shall be Type VI fluorescent pink retroreflective sheeting and shall comply with the MUTCD. Temporary traffic control signs for incident management shall be placed to notify motorists of upcoming incidents on the roadway, and shall be removed from public view once the incident has been managed. Physical properties shall comply with ASTM D 4956. Photometric properties shall be as follows.

(1) Retroreflectivity: Minimum Coefficients of Retroreflection shall be as specified in Table 1015-1.

Table 1015-1
Coefficients of Retroreflection for Fluorescent Pink Sheeting¹

Observation Angle, degrees	Entrance Angle, degrees	Fluorescent Pink
0.2	-4	100
0.2	+30	40
0.5	-4	40
0.5	+30	15

¹Minimum Coefficient of Retroreflection (R_A) ($\text{cd lx}^{-1} \text{m}^{-2}$)

(2) Color and Daytime Luminance: Color Chromaticity Coordinates and Daytime Luminance Factors shall be as specified in Table 1015-2.

Table 1015-2
Fluorescent Pink Color Specifications Limits (Daytime)

Chromaticity Coordinates (corner points) ¹								Luminance Factor, min.
1		2		3		4		Y%
x	y	x	y	x	y	x	y	25
0.450	0.270	0.590	0.350	0.644	0.290	0.536	0.230	

¹The four pairs of chromaticity coordinates measured with CIE 2° Standard Observer and 45/0 (0/45) geometry and CIE D65 Standard Illuminant.

(c) Adhesive Classes: The adhesive required for retroreflective sheeting shall be Class 1 (pressure sensitive) as specified in ASTM D 4956.

(d) Accelerated Weathering: Reflective sheeting, when processed, applied and cleaned in accordance with the manufacturer's recommendations shall perform in accordance with the accelerated weathering standards in Table 1015-3.

Table 1015-3
Accelerated Weathering Standards¹

Type	Retroreflectivity ²				Colorfastness ³	
	Orange/ Fluorescent Orange		All colors, except orange/Fluorescent Orange		Orange/ Fluorescent Orange	All colors, except orange/Fluorescent Orange
III	1 year	80 ⁴	3 years	80 ⁴	1 year	3 years
III (for drums)	1 year	80 ⁴	1 year	80 ⁴	1 year	1 year
VI	1/2 year	50 ⁵	1/2 year	50 ⁵	1/2 year	1/2 year
X	1 year	80 ⁶	3 years	80 ⁶	1 year	3 years

¹At an angle of 45° from the horizontal and facing south in accordance with ASTM G 7 at an approved test facility in Louisiana or South Florida.

²Percent retained retroreflectivity of referenced table after the outdoor test exposure time specified.

³Colors shall conform to the color specification limits of ASTM D 4956 after the outdoor test exposure time specified.

⁴ASTM D 4956, Table 8.

⁵ASTM D 4956, Table 13.

⁶ASTM D 4956, Table 4.

(e) Expected Sign Life Data and Performance: The sheeting manufacturer shall supply expected retroreflectivity service life curves for each of the following sign sheeting colors: white, green, blue, brown, red, and yellow. The service life curves shall be plots of the 95 percent expected life plotted on an x-y graph with life years on the x-axis and retroreflectivity on the y-axis. The expected life shall account for worst case installations, equivalent to an installation in South Louisiana with the sign facing to the South. The sheeting manufacturer shall also supply a table of expected life values taken from the service life curves for Revision Number 2 to the 2003 Edition of the MUTCD minimum reflectivity requirements published in the Federal Register on December 21, 2007. Reflective sheeting for signs, when processed, applied and cleaned in accordance with the manufacturer's recommendations shall perform outdoors in accordance with the performance standards in Table 1015-4.

Table 1015-4
 Reflective Sheeting Performance Standards

Type	Retroreflectivity ¹ -- Durability ²				Colorfastness ³
	Orange/ Fluorescent Orange		All colors, except orange/Fluorescent Orange		
III	3 years	80 ⁴	10 years	80 ⁴	3 years
X	3 years	80 ⁵	7years	80 ⁵	3 years

¹Percent retained retroreflectivity of referenced table after installation and the field exposure time specified.

²All sheeting shall maintain its structural integrity, adhesion and functionality after installation and the field exposure time specified.

³All colors shall conform to the color specification limits of ASTM D 4956 after installation and the field exposure time specified.

⁴ASTM D4956, Table 8.

⁵ASTM D 4956, Table 4.

(f) Temporary Signs, Barricades, Channelizing Devices, Drums and Cones: Reflective sheeting for temporary signs, barricades and channelizing devices, shall meet the requirements of ASTM D 4956, Type III except that temporary warning construction signs used on the mainline of freeways and expressways shall be fluorescent orange and meet the requirements of ASTM D 4956, Type X.

Reflective sheeting for vertical panels shall meet the requirements of ASTM D 4956, Type III.

Reflective sheeting for drums shall be a minimum of 6 inches (150 mm) wide and shall meet the requirements of ASTM D 4956, Type III, and the Supplementary Requirement S2 for Reboundable Sheeting as specified in ASTM D 4956. Reflective sheeting for traffic cone collars shall meet the requirements of ASTM D 4956, Type III or Type VI.

(g) Sheeting Guaranty. The contractor shall provide the Department with a guaranty from the sheeting manufacturer stating that if the retroreflective sheeting fails to comply with the performance requirements of this subsection, the sheeting manufacturer shall do the following:

Table 1015-5
Manufacturer's Guaranty-Reflective Sheeting

Type	Manufacturer shall restore the sign face in its field location to its original effectiveness at no cost to the Department if failure occurs during the time period ¹ as specified below		Manufacturer shall replace the sheeting required to restore the sign face to its original effectiveness at no cost to the Department if failure occurs during the time period ¹ as specified below
	Orange/Fluorescent Orange	All colors, except orange/Fluorescent Orange	All colors, except orange/Fluorescent Orange
III	<3 years	<7 years	7-10 years
X	<3 years	<5 years	5-7 years

¹ From the date of sign installation.

Replacement sheeting for sign faces, material, and labor shall carry the unexpired guaranty of the sheeting for which it replaces.

The sign fabricator shall be responsible for dating all signs with the month and year of fabrication at the time of sign fabrication. This date shall constitute the start of the guaranty obligation period.

Subsection 1015.11 - Preformed Plastic Pavement Marking Tape (06/07), Pages 842 – 844.

Delete the contents of this subsection and substitute the following.

1015.11 PREFORMED PLASTIC PAVEMENT MARKING TAPE.

(a) General: Preformed plastic pavement marking tape shall be approved products listed on QPL 64 and shall comply with ASTM D4505 Retroreflectivity Level I or Level II, or DOTD Intersection Grade (as specified below), except as modified herein. The marking tape shall be Class 2 or 3. The type and color shall be in accordance with the plans and the MUTCD.

(b) Thickness: All preformed plastic pavement marking tape shall have a minimum overall thickness of 0.060 inches (1.5 mm) when tested without the adhesive.

(c) Friction Resistance: The surface of the Retroreflectivity Level II preformed plastic pavement marking tape shall provide a minimum frictional resistance value of 35 British Polish Number (BPN) when tested according to ASTM E303. The surface of the Retroreflectivity Level I and DOTD Intersection Grade preformed plastic pavement marking tape shall provide a minimum frictional resistance value of 45 BPN when tested according to ASTM E303. Values for the Retroreflectivity Level I material with a raised surface pattern as defined in ASTM D4505 are calculated by averaging values taken at downweb and at a 45 degrees angle from downweb.

(d) Retroreflective Requirements: The preformed plastic pavement marking tape shall have the minimum initial specific luminance values shown in Table 1015-7 when measured in accordance with ASTM D 4061.

Table 1015-7
Specific Luminance of Preformed Plastic Tape

Type	Observation Angle, degrees	Entrance Angle, degrees	Specific Luminance (mcd/sq m/lx)	
			White	Yellow
Retroreflectivity Level I	1.05	88.76	500	300
DOTD Intersection Grade	1.05	88.76	375	250
Retroreflectivity Level II	1.05	88.76	250	175

(e) Durability Requirements: The DOTD Intersection Grade preformed plastic pavement marking tape shall show no appreciable fading, lifting or shrinkage for a least 12 months after placement when placed in accordance with the manufacturer's recommended procedures on pavement surfaces having a daily traffic count not to exceed 15,000 ADT per lane.

The Retroreflectivity Level I preformed plastic pavement marking tape shall show no appreciable fading, lifting or shrinkage for a least 4 years after placement for longitudinal lines and at least 2 years after placement for symbols and legends.

The Retroreflectivity Level I preformed plastic pavement marking tape shall also retain the following reflectance values for the time period detailed in Table 1015-8.

Table 1015-8
Retained Specific Luminance for Retroreflectivity Level I
Preformed Plastic Pavement Marking Tape

Time	Observation Angle, degrees	Entrance Angle, degrees	Specific Luminance (mcd/sq m/lx)	
			White	Yellow
1 year	1.05	88.76	400	240
4 years (2 years for symbols and legend)	1.05	88.76	100	100

(f) Plastic Pavement Marking Tape Guaranty (DOTD Intersection Grade and Retroreflectivity Level I): If the plastic pavement marking tape fails to comply with the performance and durability requirements of this subsection within 12 months for DOTD Intersection Grade and 4 years for Retroreflectivity Level I, the manufacturer shall replace the plastic pavement marking material at no cost to the Department.

SECTION 1020 – TRAFFIC SIGNALS:

Subsection 1020.01 – Traffic Signal Heads (06/07), Pages 873 – 884.

Delete the contents of Heading (a), General Requirements and substitute the following.

Supplemental Specifications (August 2008)

Page 30 of 30

(a) General Requirements: Traffic signal sections, beacon sections and pedestrian signal sections shall be of the adjustable type. Materials and construction of each section shall be the same.

Signals shall be constructed for either 8 or 12-inch (200 mm or 300 mm) lens in accordance with the plans. Signal sections shall have three to five sections per face and beacon sections have only one section per face. Signal sections and associated brackets shall be finished inside and out with two coats of high grade dark olive green enamel, color number 14056 according to Federal Standard No. 595b with each coat independently baked. Visors shall be coated green on the outside and black on the inside. Edges shall be deburred and smooth with no sharp edges.

Subsection 1020.04 – Poles for Traffic Signal Systems (06/07). Pages 890 – 894.

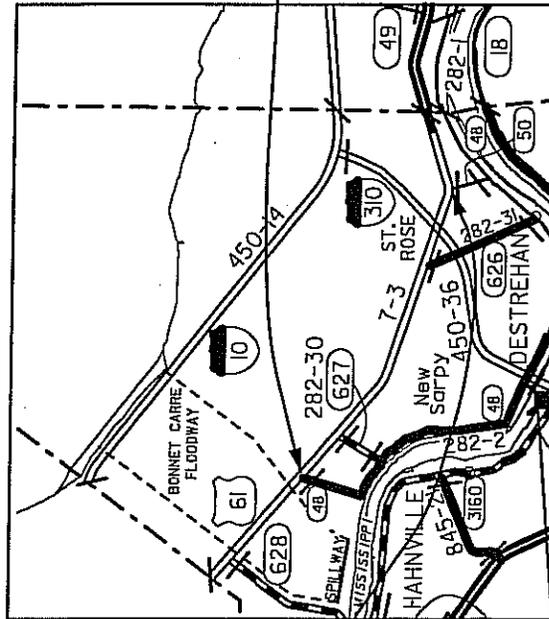
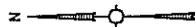
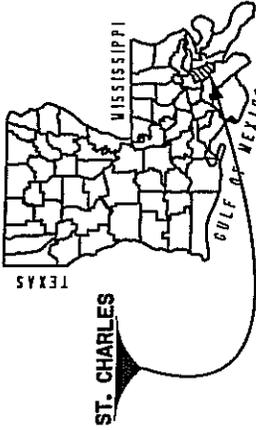
Delete the sixth paragraph of Heading (a), Pedestal Support Signal Poles, and substitute the following.

Pedestals shall be finished with at least one coat of rustproofing primer, applied to a clean surface and one coat of dark olive green enamel, color number 14056 according to Federal Standard No. 595b.

STATE OF LOUISIANA
DEPARTMENT OF TRANSPORTATION
PLANS OF PROPOSED
STATE HIGHWAY

STANDARD DESCRIPTION	PLANS	REVISION DATE
EC-01		10-1-08
GD-200		1-26-09
QR-2102		1-26-09

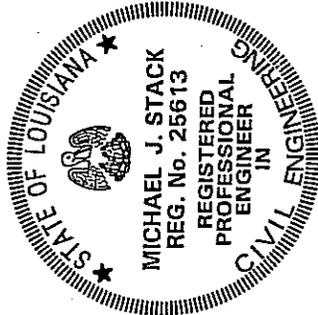
STATE PROJECT NO. 007-03-0056
US 61 BARRIER SYSTEM (LA 50 TO LA 48)
 US 61
 ST. CHARLES



STATION 0+00.00
 C.S. LOG MILE 1.68
 BEG. S.P. 007-03-0056

TRAFFIC DATA
 2007 A.D.T. = 22,200
 2017 A.D.T. = 24,500
 D = 55%
 K = 10%
 T = 12%

TYPE OF CONSTRUCTION:
 INSTALLATION OF GUARDRAIL AND ASPHALT STRIP, AND RELATED WORK



STATION 360+09.60
 C.S. LOG MILE 8.50
 END. S.P. 007-03-0056

RECOMMENDED FOR APPROVAL

W. Bradford 3/13/09
 ASSISTANT DISTRICT 02 ADMINISTRATOR-ENGINEERING

Michael J. Stack 3/13/09
 DISTRICT 02 ADMINISTRATOR

W. Terry 4-3-09
 CHIEF ENGINEER

NOTE:
 THE 2006 LOUISIANA DOTD STANDARD SPECIFICATIONS FOR ROADS AND BRIDGES, AS AMENDED BY THE PROJECT SPECIFICATIONS, SHALL GOVERN ON THIS PROJECT.

NO.	DATE	REVISION DESCRIPTION	DATE	RECOMMENDED	DATE	APPROVED
1	4-21-09	General Revision (All sheets)	4-21-09	<i>W.B.</i>	4-21-09	<i>W.B.</i>

SCHEDULE OF REVISIONS

US-61 BARRIER SYSTEM
(LA-50 TO LA-48)



DISTRICT 02
 DESIGN

DESIGNED	JMS	DATE	3/13/09
CHECKED	MJS	DATE	
DETAILED	JMS	DATE	
CHECKED	MJS	DATE	

PARISH	ST CHARLES
FEDERAL PROJECT	
STATE PROJECT	007-03-0056

BY: _____
 REVISION DESCRIPTION: _____

NO.	DATE	REVISION DESCRIPTION

DESIGNED	JMS	DATE	3/13/09
CHECKED	MJS	DATE	
DETAILED	JMS	DATE	
CHECKED	MJS	DATE	



DATE: _____

BY: _____

DATE: _____

BY: _____

DATE: _____

BY: _____

INDEX TO SHEETS

SHEET NO.	DESCRIPTION
1	TITLE SHEET
10	INDEX SHEET
2	TYPICAL SECTION
20	GENERAL CONSTRUCTION NOTES
2b	GUARDRAIL BARRIER MARKER DETAIL
3 - 3b	GUARDRAIL AND ASPHALT TABLES
4 - 4c	SUMMARY OF ESTIMATED QUANTITIES
5 - 5j	PLAN VIEWS
6 - 6e	TRAFFIC CONTROL DETAILS

LOCATION DESCRIPTION

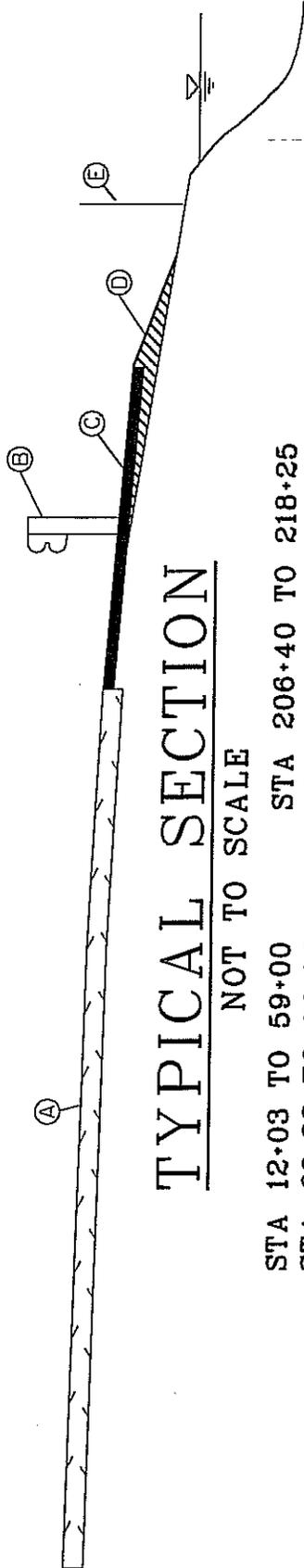
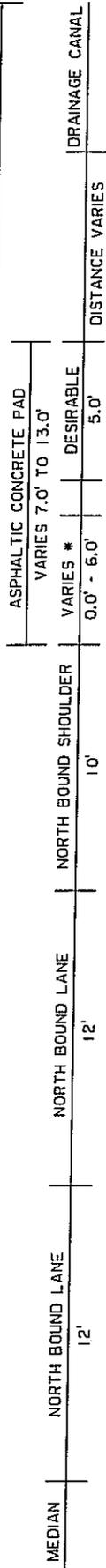
THE PROJECT BEGINS ON US 61 CONTROL SECTION 007-03 AT ITS JUNCTION WITH LA 50 (ALMEDIA RD) AT C.S.L.M. 1.68 AND PROCEEDS NORTHBOUND ALONG US 61 TO ITS JUNCTION WITH LA 48 (APPLE ST) AT C.S.L.M. 8.50.

SCOPE OF PROJECT

THE SCOPE OF THIS PROJECT WILL BE TO INSTALL A BARRIER SYSTEM ALONG THE NORTH BOUND SHOULDER OF US 61.

				US-61 BARRIER SYSTEM (LA-50 TO LA-48) INDEX SHEET		SHEET NO. 10	
DESIGNED	JMS	PARISH	ST CHARLES	DESIGNED	JMS	DATE	3/30/09
CHECKED	JMS	FEDERAL PROJECT		CHECKED	JMS	BY	
DETAILS	JMS	STATE PROJECT	007-03-0056	DATE	3/30/09	REVISION DESCRIPTION	
CHECKED	JMS			NO.	DATE		

APPARENT RIGHT OF WAY



TYPICAL SECTION

NOT TO SCALE

STA 12+03 TO 59+00	STA 206+40 TO 218+25
STA 60+28 TO 79+79	STA 219+08 TO 229+28
STA 80+05 TO 132+50	STA 230+28 TO 252+08
STA 132+78 TO 186+10	STA 252+37 TO 321+00
STA 186+51 TO 206+09	

- (A) EXIST. ROADWAY TO REMAIN
- (B) REQ'D GUARDRAIL SHALL BE INSTALLED IN ACCORDANCE WITH PLANS, GR-200, GR-202, AND AS DIRECTED BY THE PROJECT ENGINEER.
- (C) REQ'D 4" THICK ASPHALTIC CONCRETE PAD TO BE INSTALLED IN ACCORDANCE WITH PLANS, GR-200, AND AS DIRECTED BY THE PROJECT ENGINEER. THE ASPHALT PAD SHALL HAVE A MAX SLOPE OF 10H:1V. EXACT LIMITS OF THE PAD SHALL BE DETERMINED IN THE FIELD BY THE PROJECT ENGINEER. PAYMENT SHALL BE INCLUDED IN ITEM 502-01-00200. COMPACTION SHALL BE AS DIRECTED AND APPROVED BY THE PROJECT ENGINEER.
- (D) THE CONTRACTOR SHALL UTILIZE THE MATERIAL EXCAVATED FOR THE ASPHALTIC CONCRETE PAD AS BORROW, AT NO COST TO THE DEPARTMENT. THE BORROW SHALL BE SLOPED AND COMPACTED AS DIRECTED AND APPROVED BY THE PROJECT ENGINEER. ANY EXCESS REMAINING MATERIAL SHALL BECOME PROPERTY OF THE CONTRACTOR
- (E) TEMPORARY SILT FENCING SHALL BE PLACE ALONG THE BANK OF THE DRAINAGE CANAL WHERE THE CONTRACTOR IS CONDUCTING HIS OPERATIONS THROUGHOUT THE LIMITS OF CONSTRUCTION. PAYMENT SHALL BE INCLUDED IN ITEM 204-06-00100.

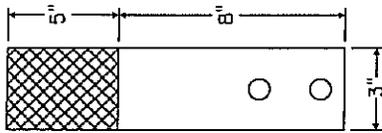
* REFER TO PLAN SHEETS AND GUARD RAIL OFFSET TABLE FOR ADDITIONAL INFORMATION.

				DESIGNED JAG CHECKED JMS	PARISH ST CHARLES	SHEET NO. 2
US-61 BARRIER SYSTEM (LA-50 TO LA-48)		TYPICAL SECTION		DETAILED JAG CHECKED JMS	FEDERAL PROJECT	
		NO. DATE	REVISION DESCRIPTION	DATE 3/30/09	STATE PROJECT 007-03-0056	
DISTRICT DESIGN				BY SHEET		

GENERAL CONSTRUCTION NOTES

1. ALL SIGNING AND TRAFFIC CONTROL SHALL CONFORM TO TRAFFIC CONTROL DETAILS AND THE MUTCD AND AS DIRECTED BY THE PROJECT ENGINEER. PAYMENT SHALL BE INCLUDED IN ITEM 713-01-00100.
2. ANY DAMAGES TO EXIST. ROADWAY, SHOULDERS, FORESLOPES, GUARDRAIL, SIGNS, ETC. BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AS DIRECTED BY THE PROJECT ENGINEER AND AT NO COST TO THE DEPARTMENT.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE IMMEDIATE REMOVAL OF ANY DEBRIS TRACKED ONTO THE SHOULDER OR TRAVEL LANES AT NO COST TO THE DEPARTMENT.
4. THE CONTRACTOR SHALL FOLLOW THE PLAN SHEETS AND TYPICAL SECTION FOR THE PLACEMENT OF THE GUARDRAIL AND ASPHALTIC CONCRETE PAD.
5. THE CONTRACTOR IS REQUIRED TO CONTACT LOUISIANA ONE CALL AND ST CHARLES PARISH GOVERNMENT (SEWER AND WATER) TO VERIFY ANY UTILITIES IN THE AREA. ANY DAMAGE DONE TO UTILITIES SHALL BE REPAIRED AT NO COST TO THE DEPARTMENT AND TO THE SATISFACTION OF THE PROJECT ENGINEER.
6. THE CONTRACTOR SHALL UTILIZE THE MATERIAL EXCAVATED FOR ASPHALTIC CONCRETE PAD AS BORROW TO ACHIEVE AN EMBANKMENT WITH A 10H : 1V SLOPE PRIOR TO PLACING ASPHALTIC CONCRETE PAD. ALL MATERIAL NOT UTILIZED ON THE JOB, SHALL BE DISPOSED OF BY THE CONTRACTOR, OUTSIDE DOTD ROW, AND AT NO COST TO THE DEPARTMENT.
7. THE CONTRACTOR SHALL BE REQUIRED TO USE A 15:1 FLARE RATE WHEN TRANSITIONING GUARDRAIL BETWEEN VARYING OFFSETS.
8. THE CONTRACTOR SHALL FIELD VERIFY CLEARANCES FOR GUARD RAIL POST INSTALLATION AT THE DRAINAGE CROSSINGS. THE CONTRACTOR SHALL INSTALL THE POSTS IN ACCORDANCE WITH GR-202 AS DIRECTED BY THE PROJECT ENGINEER AT THE DRAINAGE CROSSING LOCATIONS.
9. THE CONTRACTOR SHALL INSTALL THE GUARD RAIL IN ACCORDANCE WITH THE BLOCKOUT DETAIL FOR ALL POSTS EMBEDDED IN THE ASPHALTIC CONCRETE PAD AS SPECIFIED IN STANDARD PLAN GR-200 SHEET 10.
10. THE DEPARTMENT SHALL TEST MATERIAL EXCAVATED BY THE CONTRACTOR. IF THE PROJECT ENGINEER DETERMINES THE MATERIAL UNSUITABLE, THE CONTRACTOR SHALL USE ITEM 203-07-00100 AS BORROW. ITEM 203-07-00100 SHALL ONLY BE USED IF DIRECTED BY THE PROJECT ENGINEER. THE CONTRACTOR SHALL ONLY BE PAID FOR CUBIC YARDS DELIVERED AND UTILIZED ON THE JOB.
11. THE CONTRACTOR SHALL REMOVE SIGNS WITHIN WORK AREA IN ORDER TO PERFORM WORK. THE CONTRACTOR SHALL REPLACE SIGNS AS DIRECTED BY THE PROJECT ENGINEER WHEN WORK WITHIN THE AREA IS COMPLETE. PAYMENT SHALL BE INCLUDED IN ITEM 202-03-38000.

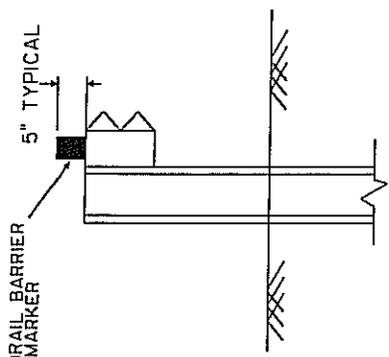
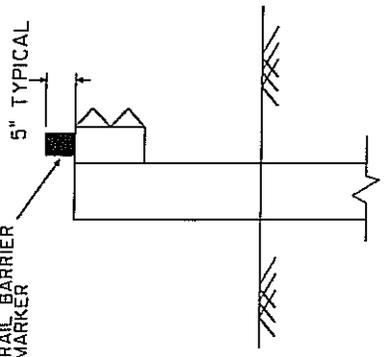
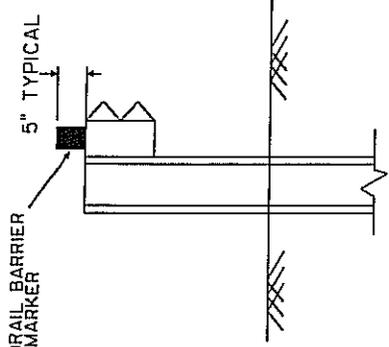
 DISTRICT DESIGN R	US-61 BARRIER SYSTEM (LA-50 TO LA-48) GENERAL CONSTRUCTION NOTES		<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 15%;">DESIGNED</td> <td style="width: 15%;">JAG</td> <td style="width: 15%;">PARISH</td> <td style="width: 15%;">ST CHARLES</td> <td style="width: 15%;">SHEET NO.</td> <td style="width: 15%;">26</td> </tr> <tr> <td>CHECKED</td> <td>MS</td> <td>FEDERAL PROJECT</td> <td></td> <td></td> <td></td> </tr> <tr> <td>DATE</td> <td>4/20/09</td> <td>STATE PROJECT</td> <td></td> <td></td> <td></td> </tr> <tr> <td>BY</td> <td></td> <td>PROJECT</td> <td></td> <td></td> <td></td> </tr> </table>	DESIGNED	JAG	PARISH	ST CHARLES	SHEET NO.	26	CHECKED	MS	FEDERAL PROJECT				DATE	4/20/09	STATE PROJECT				BY		PROJECT			
DESIGNED	JAG	PARISH	ST CHARLES	SHEET NO.	26																						
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DATE	4/20/09	STATE PROJECT																									
BY		PROJECT																									
		NO. DATE REVISION DESCRIPTION																									



GUARDRAIL BARRIER MARKER

GUARDRAIL BARRIER MARKER

GUARDRAIL BARRIER MARKER



GUARDRAIL BARRIER MARKER ATTACHED TO WOODBLOCK W/ WOOD POST

GUARDRAIL BARRIER MARKER ATTACHED TO WOODBLOCK W/ WOOD POST

GUARDRAIL BARRIER MARKER ATTACHED TO WOODBLOCK W/ STEEL POST

NOTES:

1. GUARDRAIL BARRIER MARKERS SHALL BE FILTRONA GUARDRAIL BARRIER MARKERS, PART NO. 8004228302 AS MANUFACTURED BY FILTRONA EXTRUSION/DAVIDSON TRAFFIC CONTROL PRODUCTS OR APPROVED EQUAL.
2. ALL GUARDRAIL BARRIER MARKERS INSTALLED ON THE PROJECT SHALL BE OF THE SAME MODEL AND MANUFACTURE.
3. ALL GUARDRAIL BARRIER MARKERS SHALL BE APPROVED BY THE PROJECT ENGINEER PRIOR TO INSTALLATION.
4. GUARDRAIL BARRIER MARKERS SHALL BE INSTALLED ON ALL GUARDRAIL THROUGHOUT THE LIMITS OF THE JOB. THE MARKERS SHALL BE PLACED AT 25' INTERVALS AS DIRECTED BY THE PROJECT ENGINEER.
5. REFLECTIVE SHEETING SHALL BE PLACED ON ONE SIDE FACING ONCOMING TRAFFIC. THE REFLECTIVE SHEETING SHALL BE COLORLESS WHITE TYPE III HIGH INTENSITY.
6. FASTENERS SHALL BE AS SPECIFIED BY THE MANUFACTURER AND SHALL BE GALVANIZED NAILS, SCREWS OR BOLTS. MARKERS USING A GLUE APPLICATION SHALL NOT BE USED ON THE PROJECT.
7. PAYMENT FOR GUARDRAIL BARRIER MARKERS SHALL BE INCLUDED IN NON STANDARD ITEM "GUARDRAIL BARRIER MARKER".

				DESIGNED	JMG	PARISH	ST CHARLES	SHEET	28
US-61 BARRIER SYSTEM (LA-50 TO LA-4B)				CHECKED	M/S	FEDERAL		NO.	
GUARDRAIL BARRIER MARKER DETAIL				DETAILED	JMG	PROJECT		DATE	4/13/09
				CHECKED	M/S	STATE	007-03-0056	BY	
				DATE		PROJECT		SHEET	
				REVISION DESCRIPTION					

GUARD RAIL DETAIL TABLE

STATION	STATION	DESCRIPTION	LENGTH	ITEM NUMBER	UNIT	QUANTITY
12+08.6	12+43.5	GUARDRAIL END TREATMENT (FLARED)	37.5	704-11-00100	EACH	1
12+43.5	58+73.5	BLOCKED OUT GUARD RAIL	4630.0	704-03-00100	LN FT	4630.0
58+73.5	58+98.5	GUARDRAIL TRANSITION (DOUBLE THRIE BEAM)	25.0	704-08-00200	LN FT	25.0
58+98.5	58+98.5	EXCEPTION	130.0			
60+28.5	74+72.2	BLOCKED OUT GUARD RAIL	1943.7	704-03-00100	LN FT	1943.7
74+72.2	79+78.5	GUARD RAIL ANCHOR SECTION (TRAILING END)	6.3	704-06-00100	LN FT	6.3
79+78.5	80+05.1	EXCEPTION (DRIVEWAY)	26.6			
80+05.1	80+42.6	GUARDRAIL END TREATMENT (FLARED)	37.5	704-11-00100	EACH	1
80+42.6	91+06.1	BLOCKED OUT GUARD RAIL	1063.5	704-03-00100	LN FT	1063.5
91+06.1	91+46.7	DOUBLE THRIE BEAM	40.6	704-01-02000	LN FT	40.6
91+46.7	102+58.5	BLOCKED OUT GUARD RAIL	1111.8	704-03-00100	LN FT	1111.8
102+58.5	103+39.7	DOUBLE THRIE BEAM	81.2	704-01-02000	LN FT	81.2
103+39.7	127+79.7	BLOCKED OUT GUARD RAIL	2440.0	704-03-00100	LN FT	2440.0
127+79.7	128+29.7	DOUBLE THRIE BEAM	50.0	704-01-02000	LN FT	50.0
128+29.7	132+43.2	BLOCKED OUT GUARD RAIL	413.5	704-03-00100	LN FT	413.5
132+43.2	132+49.5	GUARD RAIL ANCHOR SECTION (TRAILING END)	6.3	704-06-00100	LN FT	6.3
132+49.5	132+78.9	EXCEPTION (DRIVEWAY)	29.4			
132+78.9	133+16.4	GUARDRAIL END TREATMENT (FLARED)	37.5	704-11-00100	EACH	1
133+16.4	145+01.5	BLOCKED OUT GUARD RAIL	1185.1	704-03-00100	LN FT	1185.1
145+01.5	145+95.2	DOUBLE THRIE BEAM	93.7	704-01-02000	LN FT	93.7
145+95.2	169+54	BLOCKED OUT GUARD RAIL	2358.8	704-03-00100	LN FT	2358.8
169+54	170+87.7	DOUBLE THRIE BEAM	143.7	704-01-02000	LN FT	143.7
170+87.7	185+72.4	BLOCKED OUT GUARD RAIL	1474.7	704-03-00100	LN FT	1474.7
185+72.4	186+03.7	DOUBLE THRIE BEAM	31.3	704-01-02000	LN FT	31.3
186+03.7	186+10	GUARD RAIL ANCHOR SECTION (TRAILING END) (SINGLE THRIE BEAM)	6.3	704-06-00200	LN FT	6.3
186+10	186+51	EXCEPTION (DRIVEWAY)	41.0			
186+51	186+55.6	SPECIAL GUARDRAIL ANCHOR SECTION	6.3	NS-700-00240	EACH	1
186+55.6	188+70.2	BLOCKED OUT GUARD RAIL	214.6	704-03-00100	LN FT	214.6
188+70.2	189+45.2	DOUBLE THRIE BEAM	75.0	704-01-02000	LN FT	75.0
189+45.2	206+02.4	BLOCKED OUT GUARD RAIL	1657.2	704-03-00100	LN FT	1657.2
206+02.4	206+08.7	GUARDRAIL ANCHOR SECTION (TRAILING END)	6.3	704-06-00100	LN FT	6.3
206+08.7	206+40.9	EXCEPTION (DRIVEWAY)	32.2			
206+40.9	206+78.4	GUARDRAIL END TREATMENT (FLARED)	37.5	704-11-00100	EACH	1
206+78.4	218+24.3	BLOCKED OUT GUARD RAIL	1145.9	704-03-00100	LN FT	1145.9
218+24.3	219+07.7	DOUBLE THRIE BEAM	83.4	704-01-02000	LN FT	83.4
219+07.7	228+21.1	BLOCKED OUT GUARD RAIL	1013.4	704-03-00100	LN FT	1013.4
228+21.1	228+27.4	GUARDRAIL ANCHOR SECTION (TRAILING END)	6.3	704-06-00100	LN FT	6.3
228+27.4	229+27.4	EXCEPTION (DRIVEWAY)	101.0			
229+27.4	230+28.4	GUARDRAIL END TREATMENT (FLARED)	37.5	704-11-00100	EACH	1
230+28.4	232+11.4	BLOCKED OUT GUARD RAIL	145.5	704-03-00100	LN FT	145.5
232+11.4	232+73.9	DOUBLE THRIE BEAM	62.5	704-01-02000	LN FT	62.5
232+73.9	252+01.1	BLOCKED OUT GUARD RAIL	1827.2	704-03-00100	LN FT	1827.2
252+01.1	252+07.4	GUARDRAIL ANCHOR SECTION (TRAILING END)	6.3	704-06-00100	LN FT	6.3
252+07.4	252+37.3	EXCEPTION (DRIVEWAY)	29.9			
252+37.3	252+41.6	SPECIAL GUARDRAIL ANCHOR SECTION	6.3	NS-700-00240	EACH	1
252+41.6	252+64.8	BLOCKED OUT GUARD RAIL	25.0	704-03-00100	LN FT	25.0
252+64.8	253+27.3	DOUBLE THRIE BEAM	62.5	704-01-02000	LN FT	62.5
253+27.3	286+71.9	BLOCKED OUT GUARD RAIL	3344.6	704-03-00100	LN FT	3344.6
286+71.9	287+19.2	DOUBLE THRIE BEAM	47.3	704-01-02000	LN FT	47.3
287+19.2	287+70.5	BLOCKED OUT GUARD RAIL	1051.3	704-03-00100	LN FT	1051.3
287+70.5	298+73.7	DOUBLE THRIE BEAM	103.2	704-01-02000	LN FT	103.2
298+73.7	320+93.7	BLOCKED OUT GUARD RAIL	2220.0	704-03-00100	LN FT	2220.0
320+93.7	321+00	GUARDRAIL ANCHOR SECTION (TRAILING END)	6.3	704-06-00100	LN FT	6.3

TOTALS

ITEM NO.	UNIT	QUANT
202-02-14500	LN FT	380
704-01-02000	LN FT	833.7
704-03-00100	LN FT	29365.8
704-06-00100	LN FT	37.8
704-06-00200	LN FT	6.3
704-08-00200	LN FT	25.0
704-11-00100	EACH	5
729-16-00300	EACH	13
NS-700-00240	EACH	2

**US-61 BARRIER SYSTEM
(LA-50 TO LA-48)**

GUARD RAIL TABLE 1 OF 2

NO. DATE REVISION DESCRIPTION

BY SHEET DATE 4/13/09 STATE PROJECT 007-03-0096



DISTRICT DESIGN



PARISH ST CHARLES

FEDERAL PROJECT

DATE 4/13/09

STATE PROJECT 007-03-0096

SHEET NO. 3

GUARD RAIL OFFSET TABLE

STATION	STATION	OFFSET FROM EDGE OF TRAVEL
12+06.	34+19.2	16
34+19.2	34+79.2	VARIES (15:1 FLARE RATE)
34+79.2	41+39.2	12
41+39.2	41+69.2	VARIES (15:1 FLARE RATE)
41+69.2	42+43.4	EDGE OF SHOULDER
42+43.4	43+33.4	VARIES (15:1 FLARE RATE)
43+33.4	58+98.5	16
60+28.5	70+28.5	EDGE OF SHOULDER
70+28.5	72+01.6	VARIES
72+01.6	79+78.5	16
80+05.1	90+16.1	16
90+16.1	91+06.1	VARIES (15:1 FLARE RATE)
91+06.1	91+46.7	EDGE OF SHOULDER
91+46.7	92+36.7	VARIES (15:1 FLARE RATE)
92+36.7	101+68.5	16
101+68.5	102+58.5	VARIES (15:1 FLARE RATE)
102+58.5	103+39.7	EDGE OF SHOULDER
103+39.7	104+29.7	VARIES (15:1 FLARE RATE)
104+29.7	114+80.	16
114+80.	115+25.	VARIES (15:1 FLARE RATE)
115+25.	119+46.6	13
119+46.6	119+91.6	VARIES (15:1 FLARE RATE)
119+91.6	126+89.7	16
126+89.7	127+79.7	VARIES (15:1 FLARE RATE)
127+79.7	128+29.7	EDGE OF SHOULDER
128+29.7	129+19.7	VARIES (15:1 FLARE RATE)
129+19.7	132+49.5	16
132+78.9	144+11.5	16
144+11.5	145+01.5	VARIES (15:1 FLARE RATE)
145+01.5	170+97.7	EDGE OF SHOULDER
170+97.7	171+87.7	VARIES (15:1 FLARE RATE)

STATION	STATION	OFFSET FROM EDGE OF TRAVEL
171+87.7	184+82.4	16
184+82.4	185+72.4	VARIES (15:1 FLARE RATE)
185+72.4	186+10.	EDGE OF SHOULDER
186+51.	189+45.2	EDGE OF SHOULDER
189+45.2	190+35.2	VARIES (15:1 FLARE RATE)
190+35.2	206+08.7	16
206+40.9	217+07.9	16
217+07.9	218+25.	VARIES (15:1 FLARE RATE)
218+25.	21+97.7	EDGE OF SHOULDER
219+07.7	219+97.7	VARIES (15:1 FLARE RATE)
219+97.7	222+16.	16
222+16.	223+06.	VARIES (15:1 FLARE RATE)
223+06.	229+27.4	EDGE OF SHOULDER
230+28.4	232+73.9	EDGE OF SHOULDER
232+73.9	233+63.9	VARIES (15:1 FLARE RATE)
233+63.9	238+75.	16
238+75.	239+20.	VARIES (15:1 FLARE RATE)
239+20.	252+07.4	13
252+37.3	253+27.3	EDGE OF SHOULDER
253+27.3	253+87.3	VARIES (15:1 FLARE RATE)
253+87.3	275+00.	14
275+00.	275+30.	VARIES (15:1 FLARE RATE)
275+30.	286+11.9	16
286+11.9	286+71.9	VARIES (15:1 FLARE RATE)
286+71.9	287+19.2	EDGE OF SHOULDER
287+19.2	287+79.2	VARIES (15:1 FLARE RATE)
287+79.2	297+40.5	16
297+40.5	297+70.5	VARIES (15:1 FLARE RATE)
297+70.5	299+09.4	14
299+09.4	299+39.4	VARIES (15:1 FLARE RATE)
299+39.4	321+00.	16

NOTE: OFFSETS ON TABLE ARE FROM EDGE OF TRAVEL TO FACE OF GUARDRAIL.



DISTRICT & DESIGN

**US-61 BARRIER SYSTEM
(LA-50 TO LA-48)**

GUARD RAIL TABLE 2 OF 2



NO. DATE

REVISION DESCRIPTION

DESIGNED **JAG**

CHECKED **M/S**

DETAILED **JAG**

CHECKED **M/S**

DATE **4/15/09**

SHEET

PARISH **ST CHARLES**

FEDERAL PROJECT

STATE PROJECT **007-03-0056**

SHEET NO. **36**

ASPHALTIC CONCRETE TABLE

STA.	STA.	DESCRIPTION	LENGTH (FEET)	4.0" SUPERPAVE ASPHALTIC CONCRETE DRIVES, TURNOUTS AND MISCELLANEOUS	
				WIDTH (FT)	TONS
12+03	34+19	ASPHALT PAD	2216.0	13	704.2
34+19	34+79	ASPHALT PAD	60.0	VARIES	16.1
34+79	41+39	ASPHALT PAD	660.0	9	145.2
41+39	41+69	ASPHALT PAD	30.0	VARIES	5.9
41+69	42+43	ASPHALT PAD	74.0	7	12.7
42+43	43+33	ASPHALT PAD	90.0	VARIES	22.0
43+33	50+74	ASPHALT PAD	741.0	13	235.5
50+74	53+41	ASPHALT PAD	267.0	VARIES	40.2
53+41	57+95	ASPHALT PAD	454.0	4	44.4
57+95	59+00	ASPHALT PAD	105.0	VARIES	30.7
59+00	60+28	EXCEPTION	128.0	---	---
60+28	70+29	ASPHALT PAD	1001.0	7	171.3
70+29	72+02	ASPHALT PAD	173.0	VARIES	33.8
72+02	78+79	ASPHALT PAD	777.0	13	246.9
78+79	80+05	EXCEPTION	26.0	---	---
80+05	90+16	ASPHALT PAD	1011.0	13	321.3
90+16	91+06	ASPHALT PAD	90.0	VARIES	22.0
91+06	91+47	ASPHALT PAD	41.0	7	7.0
91+47	92+37	ASPHALT PAD	90.0	VARIES	22.0
92+37	101+69	ASPHALT PAD	932.0	13	296.2
101+69	102+59	ASPHALT PAD	90.0	VARIES	22.0
102+59	103+40	ASPHALT PAD	81.0	7	13.9
103+40	104+30	ASPHALT PAD	90.0	VARIES	22.0
104+30	114+80	ASPHALT PAD	1050.0	13	333.7
114+80	115+25	ASPHALT PAD	45.0	VARIES	2.9
115+25	119+47	ASPHALT PAD	422.0	10	103.2
119+47	119+92	ASPHALT PAD	45.0	VARIES	2.9
119+92	126+90	ASPHALT PAD	698.0	13	221.8
126+90	127+80	ASPHALT PAD	96.0	VARIES	22.0
127+80	128+30	ASPHALT PAD	50.0	7	8.6
128+30	129+20	ASPHALT PAD	90.0	VARIES	22.0
129+20	132+50	ASPHALT PAD	330.0	13	104.9
132+50	132+78	EXCEPTION	28.0	---	---
132+78	144+12	ASPHALT PAD	1134.0	13	360.4
144+12	145+02	ASPHALT PAD	90.0	VARIES	22.0
145+02	170+98	ASPHALT PAD	2596.0	7	444.2

SUBTOTAL 4083.9

STA.	STA.	DESCRIPTION	LENGTH (FEET)	4.0" SUPERPAVE ASPHALTIC CONCRETE DRIVES, TURNOUTS AND MISCELLANEOUS	
				WIDTH (FT)	TONS
170+98	171+88	ASPHALT PAD	90.0	VARIES	22.0
171+88	184+82	ASPHALT PAD	1294.0	13	411.2
184+82	185+72	ASPHALT PAD	90.0	VARIES	22.0
185+72	186+10	ASPHALT PAD	38.0	7	6.5
186+10	186+51	EXCEPTION	41.0	---	---
186+51	189+45	ASPHALT PAD	294.0	7	50.3
189+45	190+35	ASPHALT PAD	90.0	VARIES	22.0
190+35	206+09	ASPHALT PAD	1574.0	13	500.2
206+09	206+40	EXCEPTION	31.0	---	---
206+40	217+08	ASPHALT PAD	1068.0	13	339.4
217+08	218+25	ASPHALT PAD	117.0	VARIES	28.6
218+25	219+08	EXCEPTION	83.0	---	---
219+08	219+98	ASPHALT PAD	90.0	VARIES	22.0
219+98	222+16	ASPHALT PAD	218.0	13	69.3
222+16	223+06	ASPHALT PAD	90.0	VARIES	22.0
223+06	229+28	EXCEPTION	622.0	7	106.4
229+28	230+28	EXCEPTION	100.0	---	---
230+28	232+74	ASPHALT PAD	246.0	7	42.1
232+74	233+64	ASPHALT PAD	90.0	VARIES	22.0
233+64	238+75	ASPHALT PAD	511.0	13	162.4
238+75	239+20	ASPHALT PAD	45.0	VARIES	12.7
239+20	252+08	ASPHALT PAD	1288.0	13	409.3
252+08	252+37	EXCEPTION	29.0	---	---
252+37	253+27	ASPHALT PAD	90.0	7	15.4
253+27	253+87	ASPHALT PAD	60.0	VARIES	13.2
253+87	275+00	ASPHALT PAD	2113.0	11	588.2
275+00	275+30	ASPHALT PAD	30.0	VARIES	8.8
275+30	286+12	ASPHALT PAD	1082.0	13	343.8
286+12	286+72	ASPHALT PAD	60.0	VARIES	22.0
286+72	287+19	ASPHALT PAD	47.0	7	8.0
287+19	287+79	ASPHALT PAD	60.0	VARIES	22.0
287+79	297+41	ASPHALT PAD	962.0	13	305.7
297+41	297+71	ASPHALT PAD	30.0	VARIES	8.8
297+71	299+09	ASPHALT PAD	138.0	11	37.1
299+09	299+39	ASPHALT PAD	30.0	VARIES	8.8
299+39	321+00	ASPHALT PAD	2161.0	13	686.7

SUBTOTAL 4318.9
TOTAL 8402.8

**US-61 BARRIER SYSTEM
(LA-50 TO LA-48)**

ASPHALT TABLE

DESIGNED **JAG** / CHECKED **MS**

DETAILED **JAG** / CHECKED **MS**

DATE 4/1/09 / SHEET

NO. DATE

REVISION DESCRIPTION

BY

ST CHARLES

PARISH PROJECT

FEDERAL PROJECT

STATE PROJECT

007-03-0056

DISTRICT & DESIGN

SHEET NO. 36



Summary Of Estimated Quantities

Proposal ID: 007-03-0056
Federal Project Number:

State Project Number: 007-03-0056

Proposal Description: US-61 BARRIER SYSTEM (LA-50 TO LA-48)

Item No.	Description	Supplemental Description	Alternate Set Member	Quantity	Units
202-02-14500	Removal of Guard Rail	General Items		380.000	LNFT
202-03-38000	Relocation of Sign	Various Traffic Warning Signs		34.000	EACH
203-07-00100	Borrow (Vehicular Measurement)			500.000	CUYD
204-06-00100	Temporary Silt Fencing			30,895.000	LNFT
502-01-00200	Superpave Asphaltic Concrete, Drives, Turnouts and Miscellaneous			8,402.800	TON
704-01-02000	Guard Rail (Double Thrie Beam) (3'-1 1/2" post spacing)			833.700	LNFT
704-03-00100	Blocked Out Guard Rail			29,365.800	LNFT
704-06-00100	Guard Rail Anchor Sections (Trailing End)			37.800	LNFT
704-06-00200	Guard Rail Anchor Sections (Trailing End) (Single Thrie Beam)			6.300	LNFT
704-08-00200	Guard Rail Transitions (Double Thrie Beam)			25.000	LNFT
704-11-00100	Guard Rail End Treatment (Flared)			5.000	EACH
713-01-00100	Temporary Signs and Barricades			1.000	LUMP
727-01-00100	Mobilization			1.000	LUMP
729-16-00300	Object Marker Assembly (Type 3)			13.000	EACH

Notes:

Rev. 1 - 04/21/2009 - General Revision



Summary Of Estimated Quantities

Proposal ID: 007-03-0056
Federal Project Number:

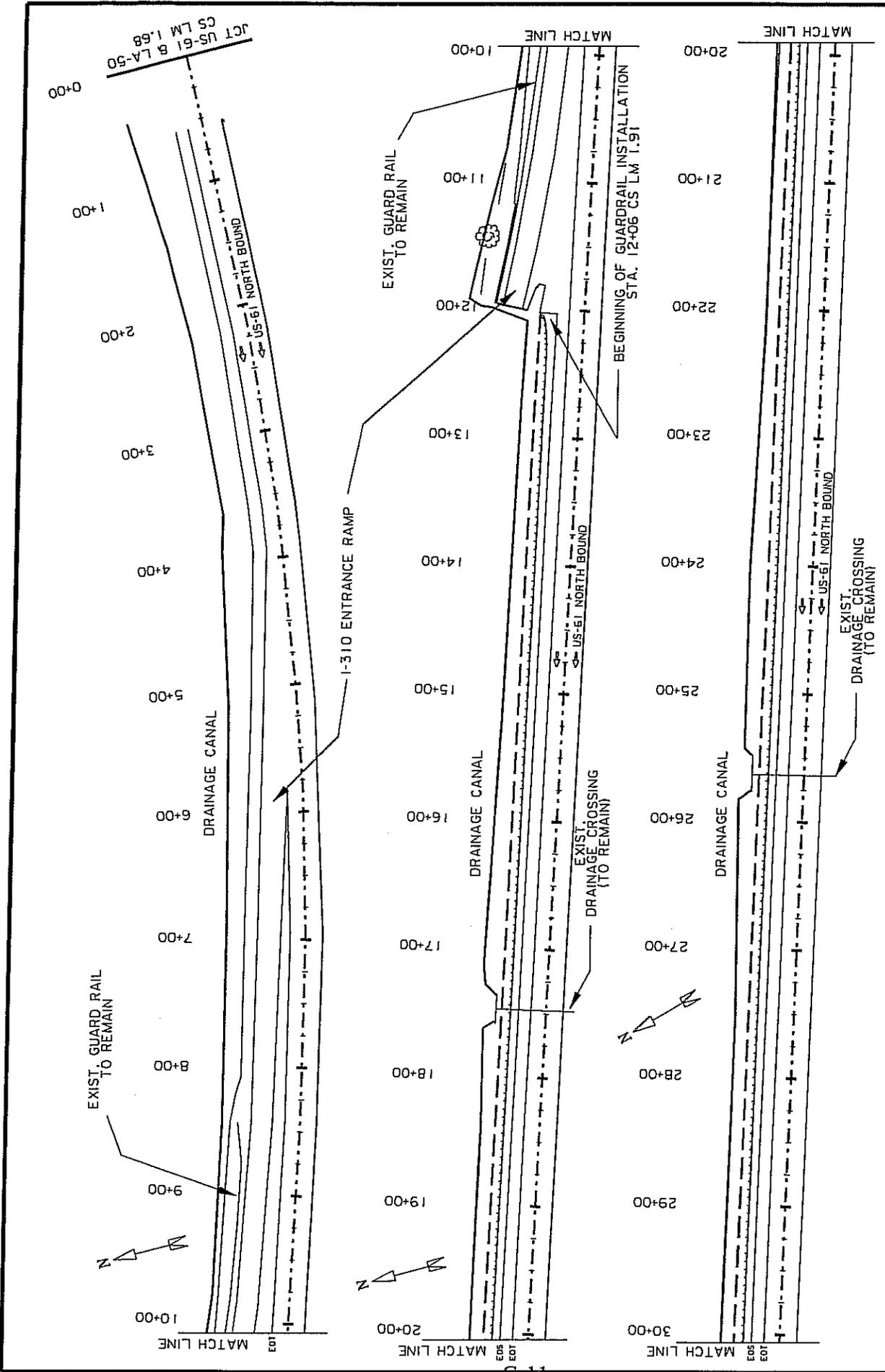
State Project Number: 007-03-0056

Proposal Description: US-61 BARRIER SYSTEM (LA-50 TO LA-48)

Item No.	Description	Supplemental Description	Alternate Set Member	Quantity	Units
740-01-00100	Construction Layout			1,000	LUMP
NS-700-00240	Special Guard Rail Anchor Section			2,000	EACH
NS-729-00002	Guardrail Barrier Marker			1,220,000	EACH

Notes:

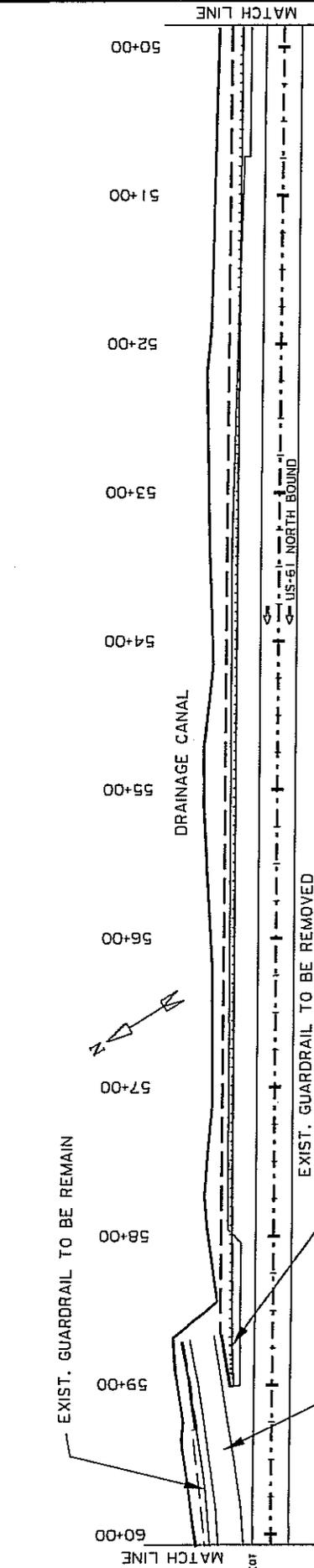
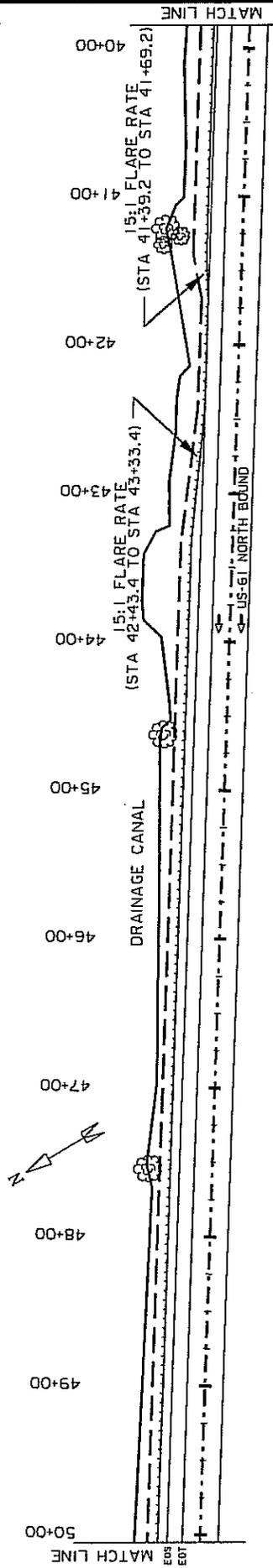
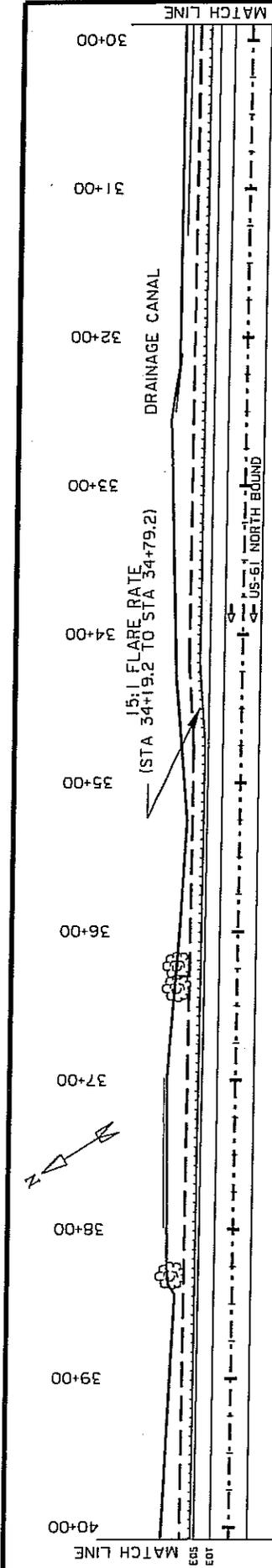
Rev. 1 - 04/21/2009 - General Revision



- NOTES:
1. SOUTH BOUND US-61 LANES NOT SHOWN ON PLAN VIEWS.
 2. SEE TYPICAL SECTION FOR ADDITIONAL INFORMATION.
 3. ALL WORK SHOWN ON PLANS IS WITHIN EXISTING ROW.

--- LIMIT OF ASPHALT CONCRETE PAD
 - - - - - RECD GUARDRAIL

		US-61 BARRIER SYSTEM (LA-50 TO LA-48)		PLAN VIEWS (SHEET 1 OF 11)	
DESIGNED CHECKED JAG MS	PARISH ST CHARLES	DETAILED CHECKED JAG MS	FEDERAL PROJECT	DATE 3/31/09	STATE PROJECT 007-03-0056
NO.			DATE		
REVISION DESCRIPTION					
SHEET NO. 5					

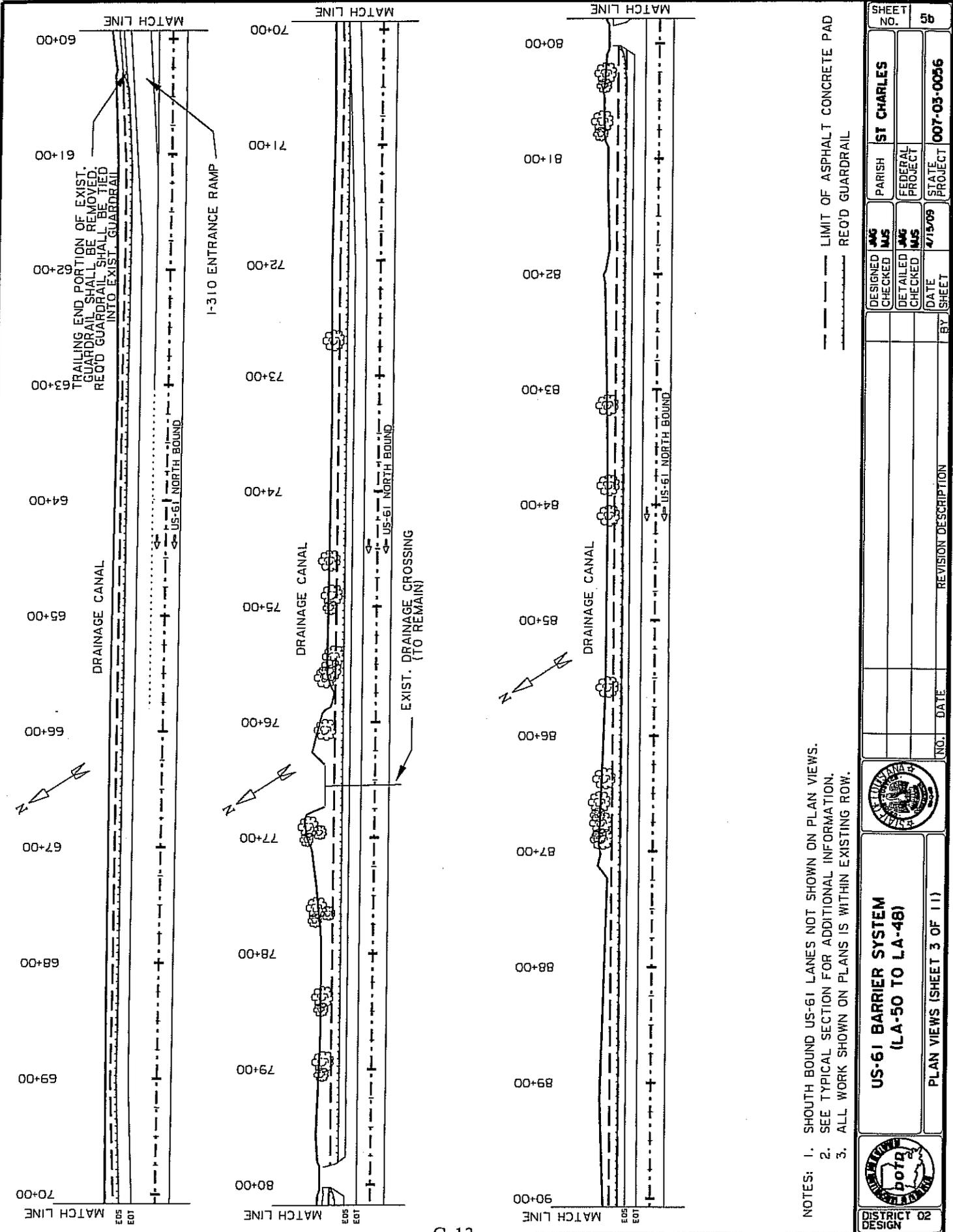


EXIST. GUARDRAIL TO BE REMOVED
 REO'D GUARD RAIL SHALL BE TIED TO
 EXIST. RAMP STRUCTURE (I-310 TO US-61)

- NOTES:
1. SHOUTH BOUND US-61 LANES NOT SHOWN ON PLAN VIEWS.
 2. SEE TYPICAL SECTION FOR ADDITIONAL INFORMATION.
 3. ALL WORK SHOWN ON PLANS IS WITHIN EXISTING ROW.

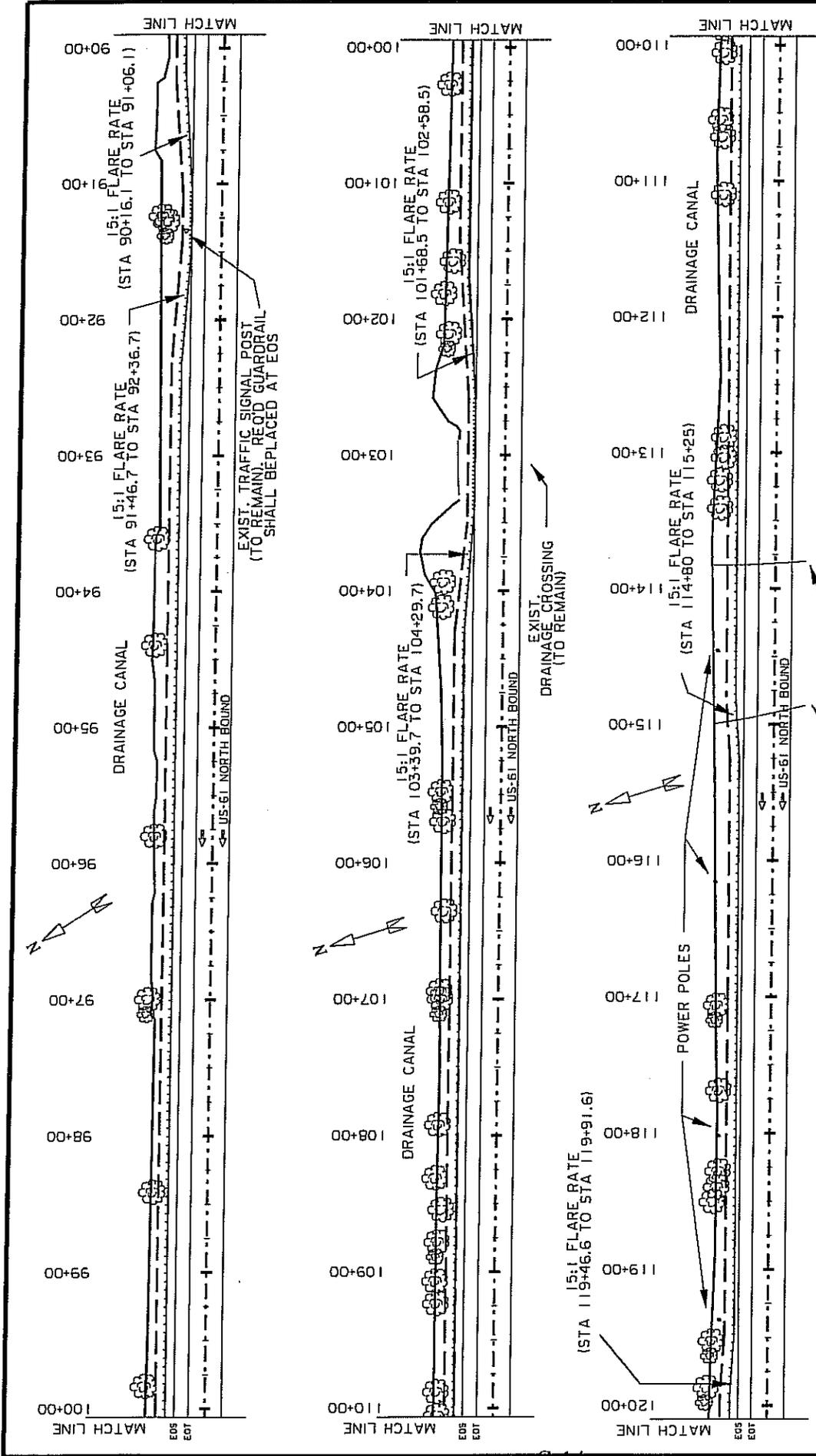
--- LIMIT OF ASPHALT CONCRETE PAD
 - - - - - REO'D GUARDRAIL

 DISTRICT DESIGN	US-61 BARRIER SYSTEM (LA-50 TO LA-48)			DESIGNED	JAG	PARISH	ST CHARLES	SHEET	50
	NO.	DATE		CHECKED	MJS	FEDERAL	PROJECT	NO.	DATE
PLAN VIEWS (SHEET 2 OF 11)				DETAILED CHECKED DATE 3/31/09	DATE 3/31/09	PROJECT 007-03-0056	PROJECT 007-03-0056	REVISION DESCRIPTION	SHEET NO.



- NOTES:
1. SOUTH BOUND US-61 LANES NOT SHOWN ON PLAN VIEWS.
 2. SEE TYPICAL SECTION FOR ADDITIONAL INFORMATION.
 3. ALL WORK SHOWN ON PLANS IS WITHIN EXISTING ROW.

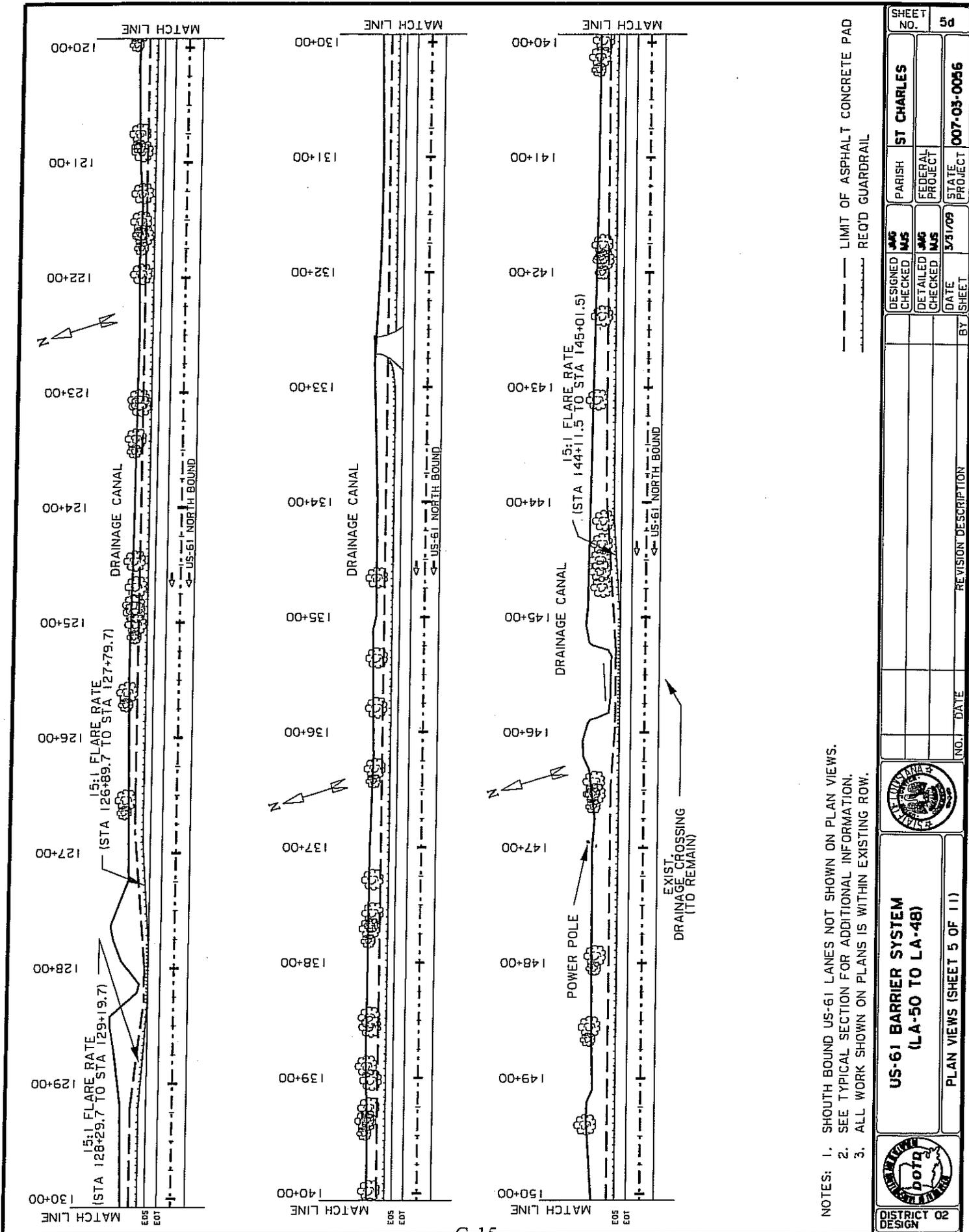
DESIGNED		JAG	MS	ST CHARLES	SHEET NO. 56	
CHECKED		JAG	MS	PARISH	FEDERAL PROJECT	
DATE		4/15/09	BY	STATE PROJECT	007-03-0056	
REVISION DESCRIPTION						
NO.		DATE				
DISTRICT DESIGN		POINT		US-61 BARRIER SYSTEM (LA-50 TO LA-48)		
PLAN VIEWS (SHEET 3 OF 11)						



--- LIMIT OF ASPHALT CONCRETE PAD
 - - - - - REOD GUARDRAIL

- NOTES:
1. SHOUTH BOUND US-61 LANES NOT SHOWN ON PLAN VIEWS.
 2. SEE TYPICAL SECTION FOR ADDITIONAL INFORMATION.
 3. ALL WORK SHOWN ON PLANS IS WITHIN EXISTING ROW.

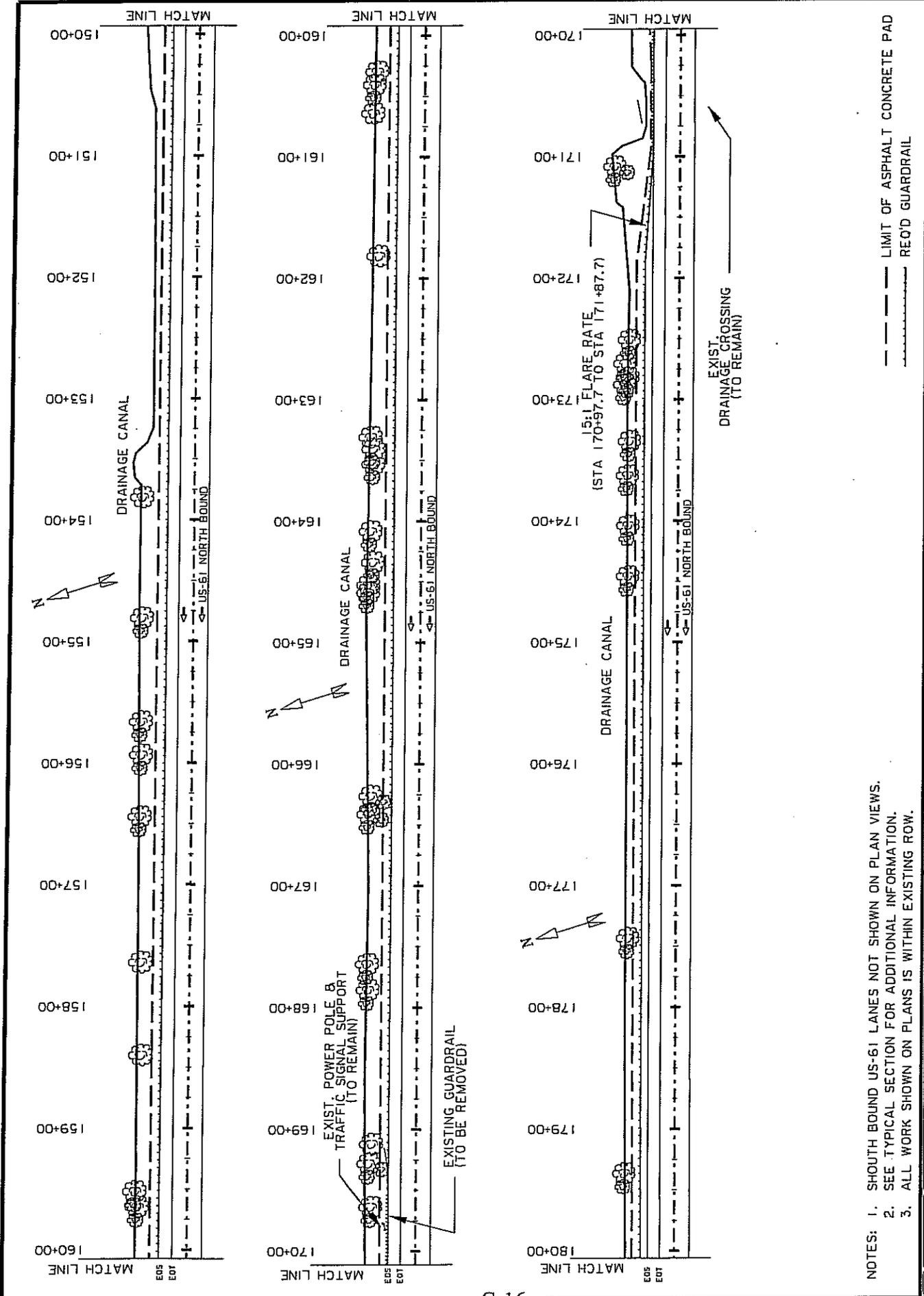
		DESIGNED JAG MS	PARISH ST CHARLES	SHEET NO. 5c
US-61 BARRIER SYSTEM (LA-50 TO LA-48)		CHECKED JAG MS	FEDERAL PROJECT	
DISTRICT DESIGN 02		DATE 3/31/09	STATE PROJECT 007-03-0056	
PLAN VIEWS (SHEET 4 OF 11)		REVISION DESCRIPTION	BY	
NO.	DATE			



--- LIMIT OF ASPHALT CONCRETE PAD
 - - - - - ROAD GUARDRAIL

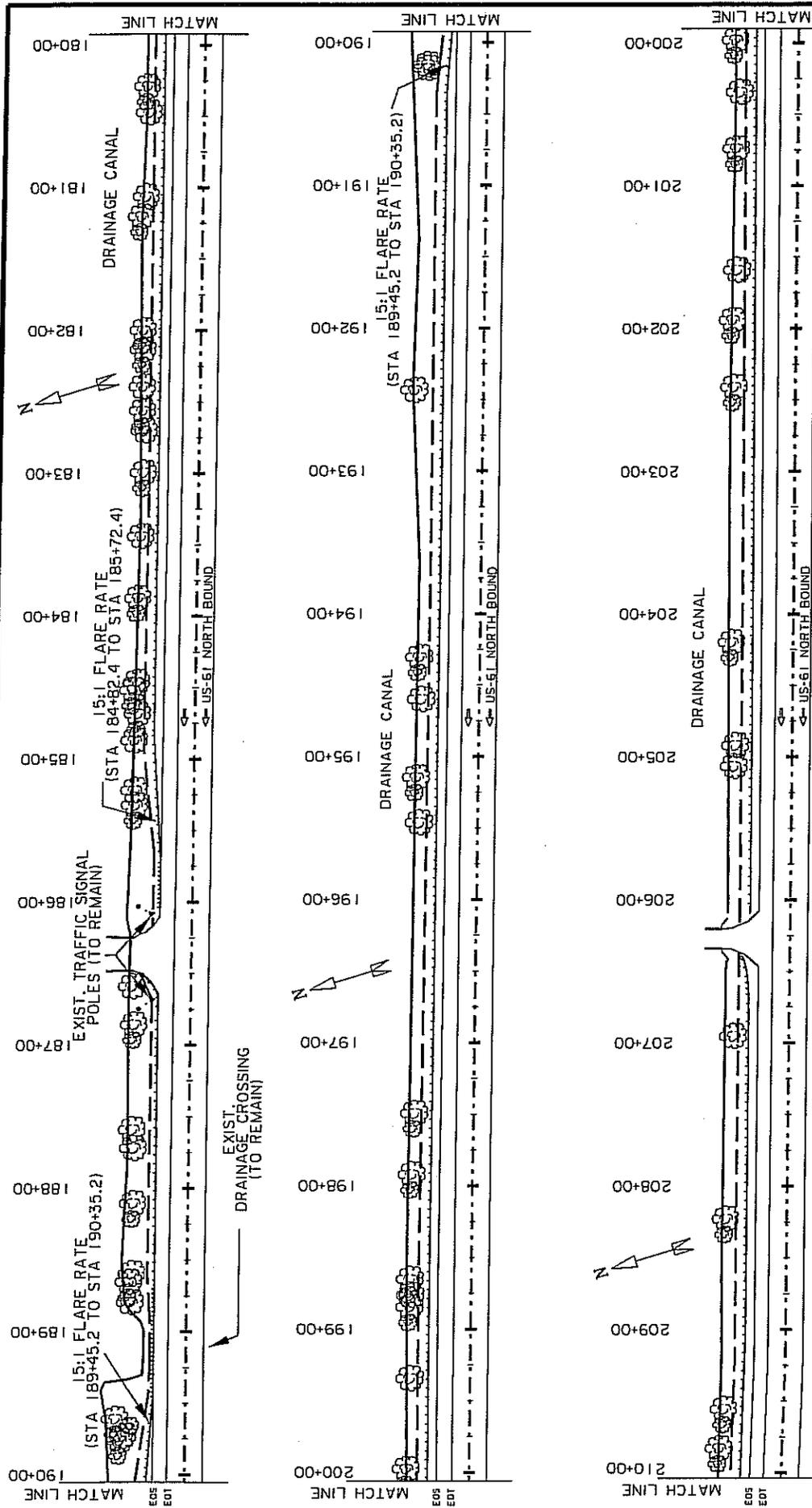
- NOTES:
1. SHOULD BOUND US-61 LANES NOT SHOWN ON PLAN VIEWS.
 2. SEE TYPICAL SECTION FOR ADDITIONAL INFORMATION.
 3. ALL WORK SHOWN ON PLANS IS WITHIN EXISTING ROW.

DISTRICT 20 DESIGN		US-61 BARRIER SYSTEM (LA-50 TO LA-48)	(NO.) DATE REVISION DESCRIPTION BY SHEET	DESIGNED	PARISH	SHEET NO.
				CHECKED	FEDERAL PROJECT	95
				JAG MJS	ST CHARLES	
				JAG MJS	FEDERAL PROJECT	
				DATE	STATE PROJECT	
				5/31/09	007-03-0056	



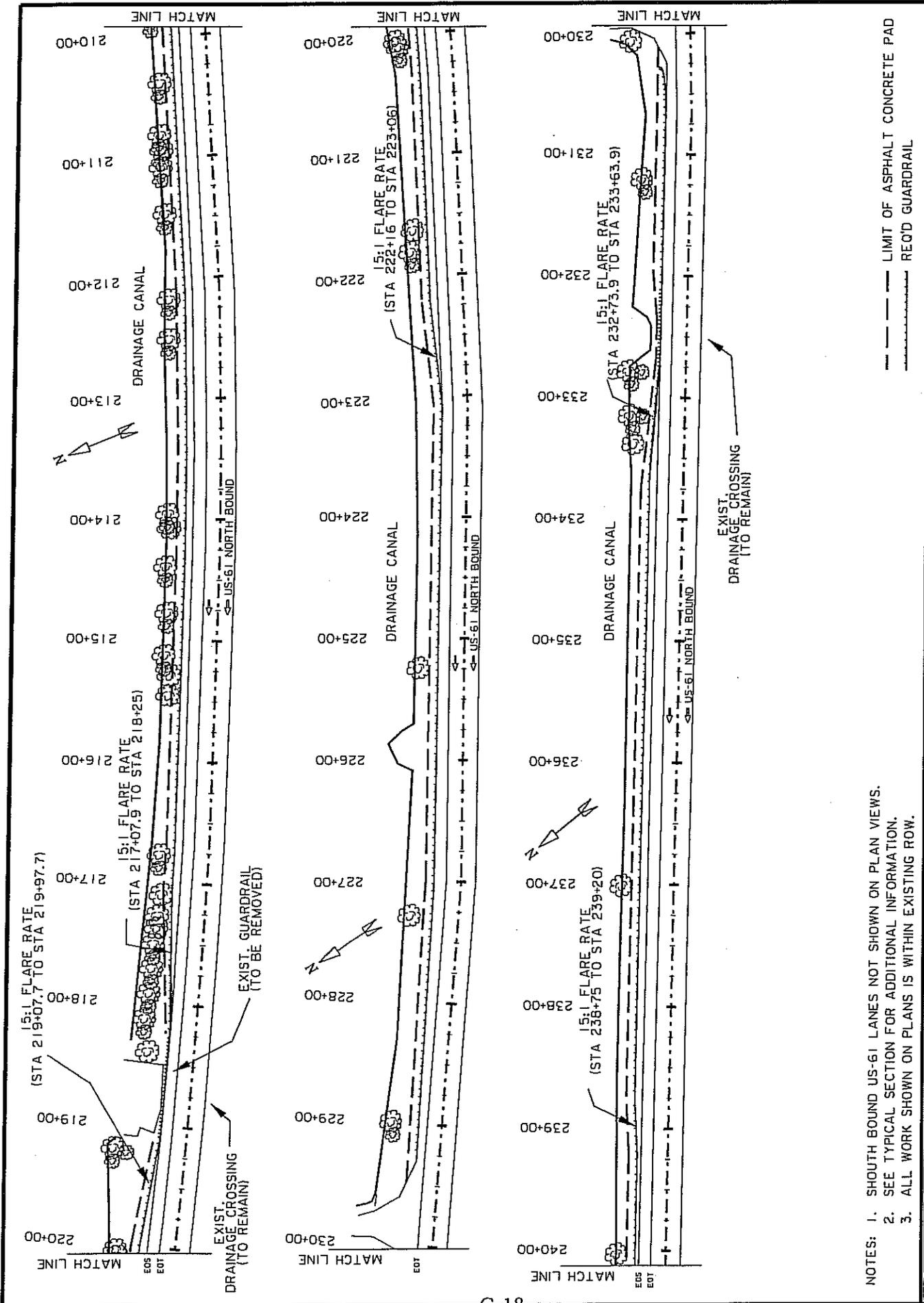
- NOTES:
1. SHOUTH BOUND US-61 LANES NOT SHOWN ON PLAN VIEWS.
 2. SEE TYPICAL SECTION FOR ADDITIONAL INFORMATION.
 3. ALL WORK SHOWN ON PLANS IS WITHIN EXISTING ROW.

DISTRICT DESIGN		NO. DATE		REVISION DESCRIPTION		BY	
US-61 BARRIER SYSTEM (LA-50 TO LA-48)							
PLAN VIEWS (SHEET 6 OF 11)							
							
DESIGNED	JAG	PARISH	ST CHARLES				
CHECKED	MJS	FEDERAL PROJECT					
DATE	4/13/09	STATE PROJECT	007-03-0056				
SHEET NO. 5e							



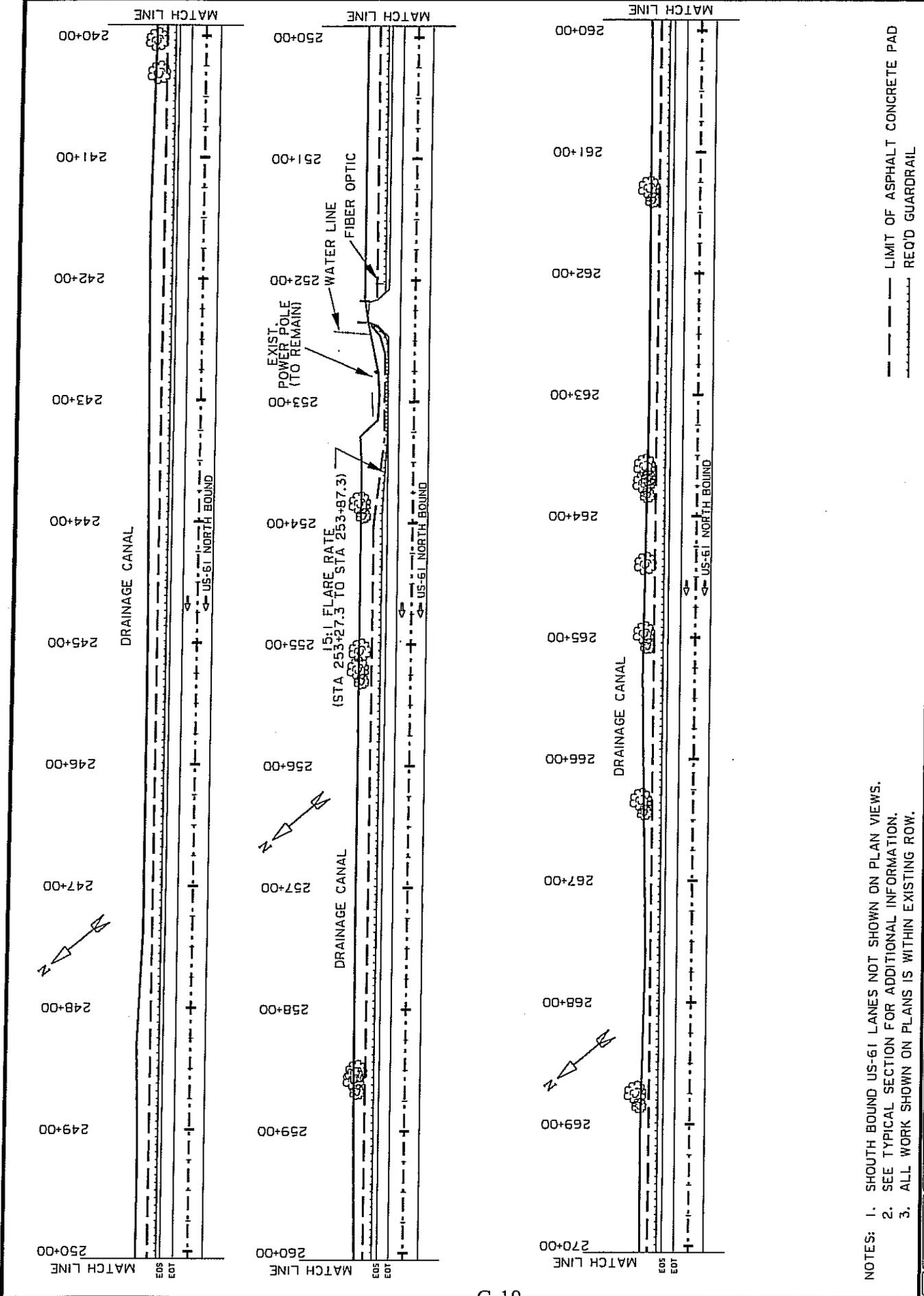
NOTES: 1. SHOUTH BOUND US-61 LANES NOT SHOWN ON PLAN VIEWS.
 2. SEE TYPICAL SECTION FOR ADDITIONAL INFORMATION.
 3. ALL WORK SHOWN ON PLANS IS WITHIN EXISTING ROW.

DISTRICT 20 DESIGN	 US-61 BARRIER SYSTEM (LA-50 TO LA-48)	NO. DATE REVISION DESCRIPTION	BY	DESIGNED	JAG	PARISH	ST CHARLES	SHEET NO.	15
				CHECKED	MJS	FEDERAL PROJECT			
				DATE	3/31/09	STATE PROJECT	007-03-0056		



- NOTES:
1. SHOULDR BOUND US-61 LANES NOT SHOWN ON PLAN VIEWS.
 2. SEE TYPICAL SECTION FOR ADDITIONAL INFORMATION.
 3. ALL WORK SHOWN ON PLANS IS WITHIN EXISTING ROW.

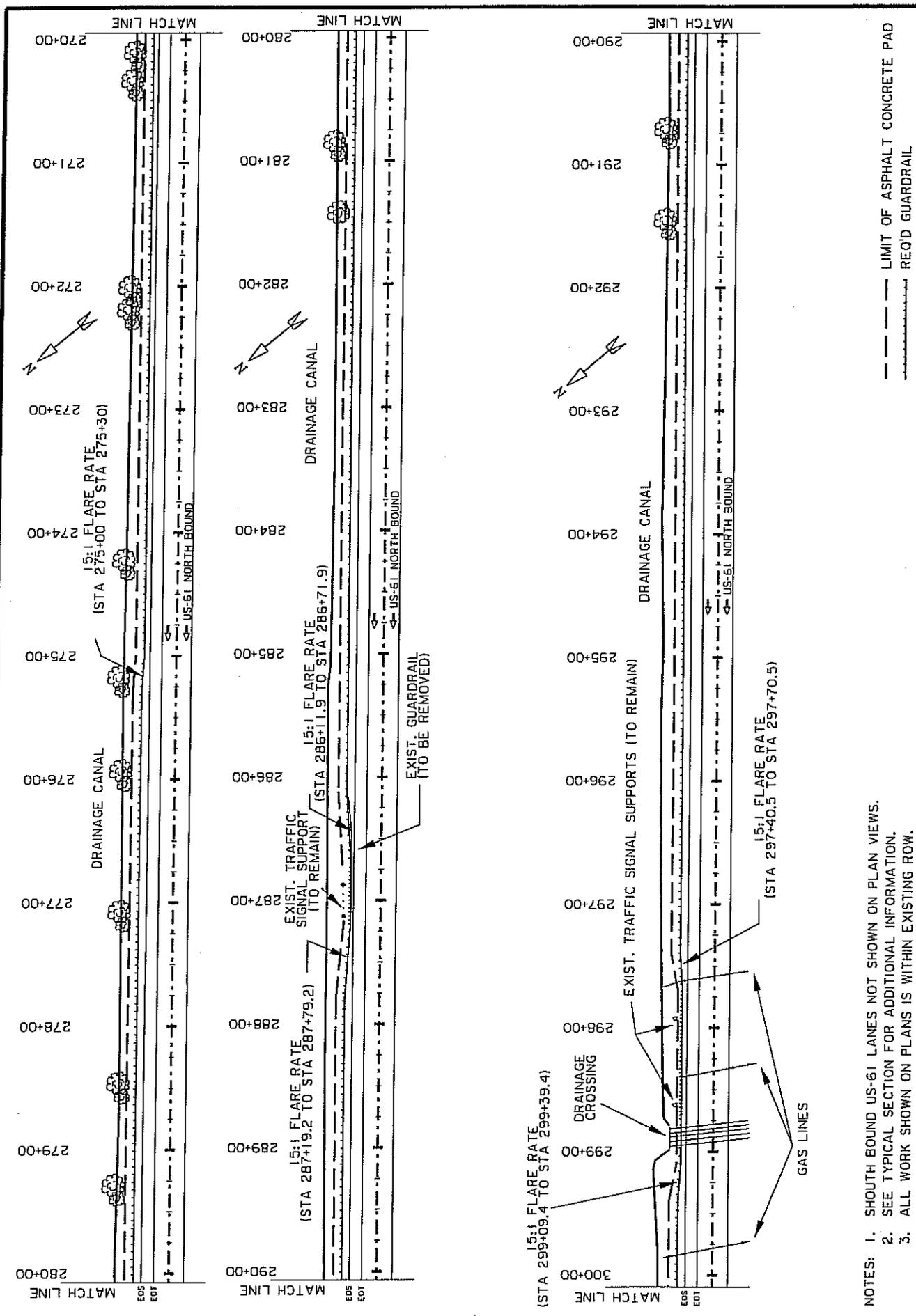
DESIGNED JMS		PARISH ST CHARLES	SHEET NO. 59
CHECKED JMS		FEDERAL PROJECT	
DATE 4/15/09		STATE PROJECT 007-03-0066	
REVISION DESCRIPTION		BY	
NO.		DATE	
		US-61 BARRIER SYSTEM (LA-50 TO LA-48)	
		PLAN VIEWS (SHEET 8 OF 11)	
DISTRICT 02 DESIGN			



- NOTES:
1. SHOUTH BOUND US-61 LANES NOT SHOWN ON PLAN VIEWS.
 2. SEE TYPICAL SECTION FOR ADDITIONAL INFORMATION.
 3. ALL WORK SHOWN ON PLANS IS WITHIN EXISTING ROW.

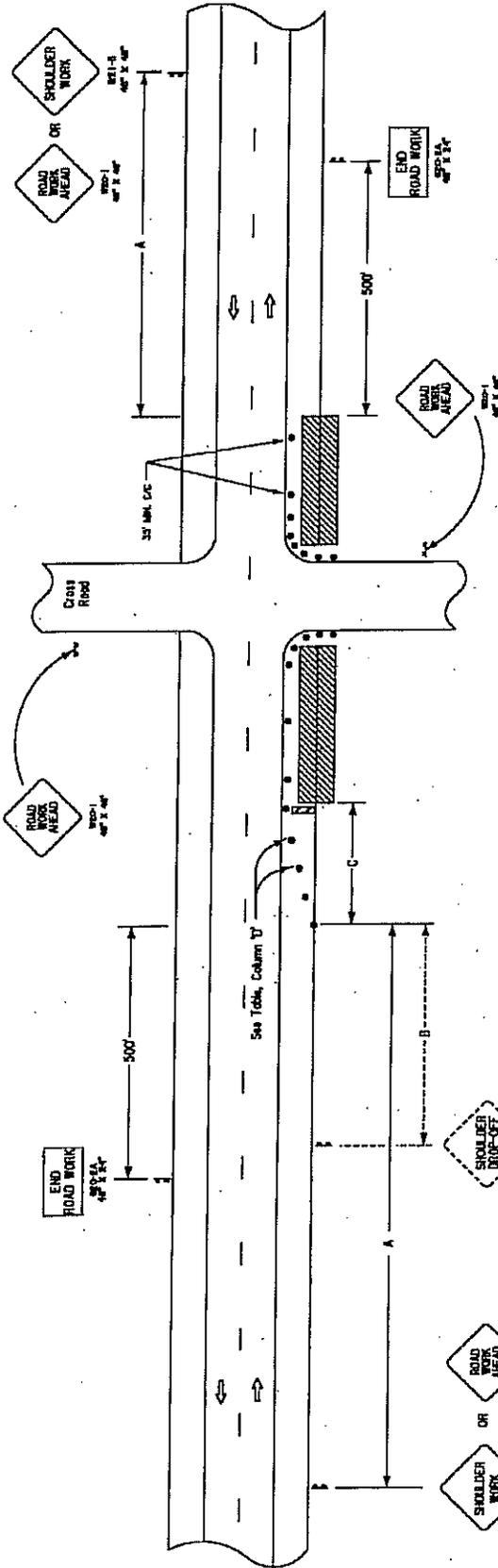
--- LIMIT OF ASPHALT CONCRETE PAD
 - - - - - REQ'D GUARDRAIL

DISTRICT DESIGN		NO. 20		DATE		REVISION DESCRIPTION		BY		SHEET NO.	
US-61 BARRIER SYSTEM (LA-50 TO LA-48)		PLAN VIEWS (SHEET 9 OF 11)		DATE		REVISION DESCRIPTION		BY		SHEET NO.	
STATE OF MISSISSIPPI		DESIGNED		CHECKED		DATE		BY		SHEET NO.	
ST CHARLES		JAG		M/S		3/31/09		ST CHARLES		56	
PARISH		JAG		M/S		FEDERAL PROJECT		FEDERAL PROJECT		007-03-0056	
STATE PROJECT		JAG		M/S		STATE PROJECT		STATE PROJECT		007-03-0056	



- NOTES:
1. SHOUTH BOUND US-61 LANES NOT SHOWN ON PLAN VIEWS.
 2. SEE TYPICAL SECTION FOR ADDITIONAL INFORMATION.
 3. ALL WORK SHOWN ON PLANS IS WITHIN EXISTING ROW.

 US-61 BARRIER SYSTEM (LA-50 TO LA-48)	NO. _____ DATE _____	REVISION DESCRIPTION _____ BY _____	DESIGNED JMS CHECKED _____	PARISH ST CHARLES	SHEET NO. 5
	DISTRICT DESIGN DOTD	PLAN VIEWS (SHEET 10 OF 11)	DETAILED CHECKED JMS DATE 4/15/09	FEDERAL PROJECT _____	STATE PROJECT 007-03-0056



LEGEND

- ▲ Traffic Sign
- Channelizing Device
- ▨ Work Area
- ▩ Type III Barricade

SPEED LIMIT	Shoulder Changes Table	
	Minimum Spacing	Maximum Spacing
35 mph	500'	100'
45 mph	1000'	200'
≥ 55 mph	1500'	300'

If horizontal curve radius is less than 300', device spacing shall be 25'.

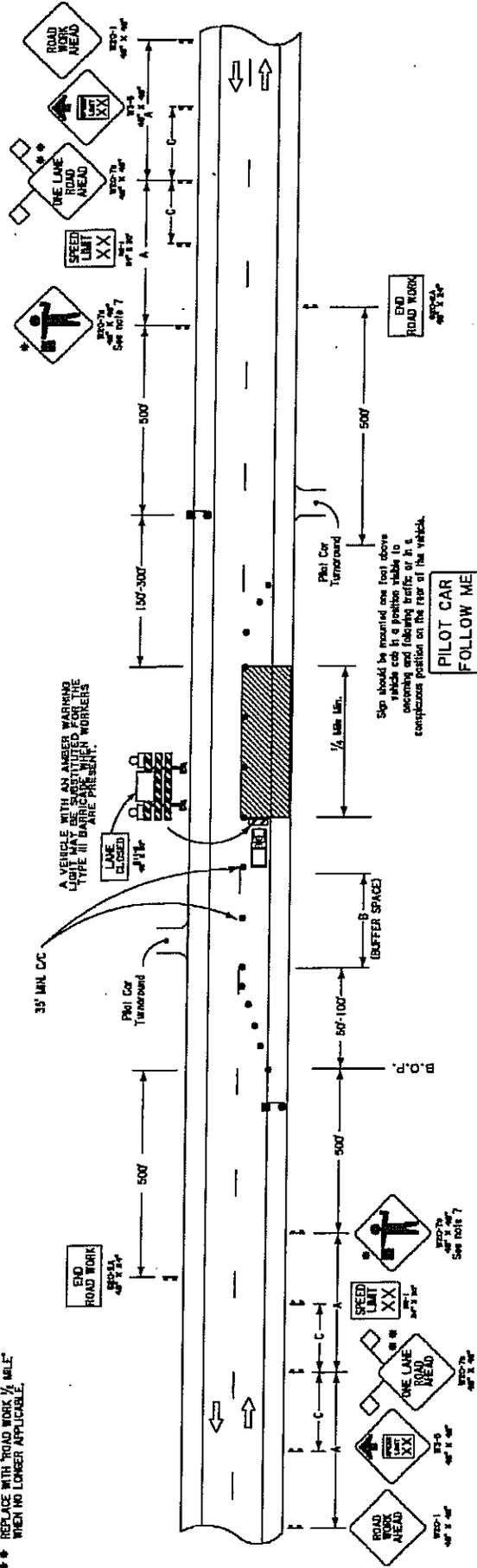
- NOTES**
THIS SHEET SHALL BE USED WITH THE "TEMPORARY TRAFFIC CONTROL GENERAL NOTES SHEET (TC-00)".
- THIS LAYOUT REPRESENTS TRAFFIC CONTROLS REQUIRED FOR WORKERS AND EQUIPMENT OPERATING WITHIN THE CLEAR ZONE FOR MORE THAN 1 HOUR, LESS THAN 1 HOUR, SEE FIG. TA-4 OF THE MUTCD. PORTABLE SIGNS MAY BE USED FOR WORK LASTING LESS THAN 3 DAYS.
 - NO SIGNS OR BARRICADES ARE REQUIRED FOR EQUIPMENT OPERATING OR WORK IN PROGRESS OUTSIDE THE CLEAR ZONE.
 - SIGNS AND BARRICADES SHALL BE COVERED OR REMOVED DURING NONWORKING HOURS UNLESS A DROP-OFF OR PHYSICAL OBSTRUCTION REMAINS WITHIN THE CLEAR ZONE.
 - TRAFFIC CONES MAY BE USED AS CHANNELIZING DEVICES ALONG THE WORK AREA DURING DAYLIGHT HOURS ONLY.
 - WORK OR EQUIPMENT CONFINED TO A SPOT LOCATION (LESS THAN 500 FEET) SHALL BE MARKED BY CHANNELIZING DEVICES SPACED AT 75 FEET ON BY A VEHICLE WITH A YELLOW FLASHING LIGHT OR BELOW OR ABOVE LIGHT VISIBLE TO ONCOMING TRAFFIC. WORK EXTENDING MORE THAN 500 FEET OF ROADWAY LENGTH SHALL BE MARKED WITH APPROPRIATE DEVICES SPACED AS NOTED IN THE TABLE.
 - TRAFFIC CONES MAY BE USED AS CHANNELIZING DEVICES ALONG THE WORK AREA DURING DAYLIGHT HOURS ONLY.
 - WORK OR EQUIPMENT CONFINED TO A SPOT LOCATION (LESS THAN 500 FEET) SHALL BE MARKED BY CHANNELIZING DEVICES SPACED AT 75 FEET ON BY A VEHICLE WITH A YELLOW FLASHING LIGHT OR BELOW OR ABOVE LIGHT VISIBLE TO ONCOMING TRAFFIC. WORK EXTENDING MORE THAN 500 FEET OF ROADWAY LENGTH SHALL BE MARKED WITH APPROPRIATE DEVICES SPACED AS NOTED IN THE TABLE.
 - MINIMUM CONSTRUCTION SIGNING. ANY ADDITIONAL SIGNS SURVIVING IN THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND REQUIRED BY THE PROJECT ENGINEER SHALL BE INSTALLED UNDER ITEM 713-01.
 - ANY SIGNS IN CONFLICT WITH CONSTRUCTION SIGNS SHALL BE REMOVED OR COVERED.
 - WHEN A WORK AREA HAS BEEN ESTABLISHED ON ONE SIDE OF THE ROADWAY ONLY, THERE SHALL BE NO CONFLICTING OPERATIONS OR PARKING ON THE OPPOSITE SHOULDER WITHIN 500 FEET OF THE WORK AREA.
 - SPEED LIMIT IN THE ABOVE TABLE REFERS TO THE LEGALLY ESTABLISHED SPEED LIMIT BEFORE CONSTRUCTION. IF WORKERS ARE PRESENT WITHIN 2' OF TRAVEL LANE, SPEED LIMIT MAY NEED TO BE REDUCED.
 - A TEMPORARY EDGE LINE OR CHANNELIZING DEVICE SHALL BE PLACED AT THE PAVEMENT EDGE ADJACENT TO THE DROP-OFF DURING NONWORKING HOURS WHEN THE DROP-OFF IS GREATER THAN 2'. A PORTABLE BARRIER SHALL BE USED.
 - IF THE SPEED LIMIT IS GREATER THAN 45 MPH AND THE DROP-OFF IS 10' OR GREATER WITHIN 2' OF THE TRAVEL LANE DURING NONWORKING HOURS, A PORTABLE BARRIER SHALL BE USED.
 - SHOULDER DROP-OFFS
 - WHEN A SHOULDER DROP-OFF IS GREATER THAN 2' BUT LESS THAN 6' EXIST, A "SHOULDER DROP-OFF" SIGN WILL FOLLOW THE "SHOULDER WORK" SIGN. WHEN THE DROP-OFF EXCEEDS 6', THE "SHOULDER DROP-OFF" SIGN SHALL BE REPLACED BY A "NO SHOULDER" SIGN.
 - IF THE SPEED LIMIT IS GREATER THAN 45 MPH AND THE DROP-OFF IS 10' OR GREATER WITHIN 2' OF THE TRAVEL LANE DURING NONWORKING HOURS, A PORTABLE BARRIER SHALL BE USED.
 - WHEN A WORK AREA HAS BEEN ESTABLISHED ON ONE SIDE OF THE ROADWAY ONLY, THERE SHALL BE NO CONFLICTING OPERATIONS OR PARKING ON THE OPPOSITE SHOULDER WITHIN 500 FEET OF THE WORK AREA.
 - ANY SIGNS IN CONFLICT WITH CONSTRUCTION SIGNS SHALL BE REMOVED OR COVERED.
 - MINIMUM CONSTRUCTION SIGNING. ANY ADDITIONAL SIGNS SURVIVING IN THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND REQUIRED BY THE PROJECT ENGINEER SHALL BE INSTALLED UNDER ITEM 713-01.

12. TYPE III BARRICADES SHALL BE PLACED IN THE CLOSED LANE AT A 1000' DISTANCE FROM THE ACTIVE WORK IS ON GOING AND THE LANE MUST REMAIN CLOSED. WHEN THE BARRICADE IS BEING MOVED BEYOND EACH OR GROUP OF UNFILLED HOLES OR HOLES FILLED WITH TEMPORARY MATERIAL, OR WHENE UNCURED CONCRETE EXISTS.



SHEET NO. 6A		PARISH ST CHARLES	
DESIGNED CHECKED	A. COLVILLE	FEDERAL PROJECT	STATE PROJECT
DETAILED CHECKED	A. COLVILLE	DATE	007-03-0056
NO. DATE		BY	
NO.		REVISION DESCRIPTION	
TC-01		TRAFFIC CONTROL LAYOUT FOR WORK LESS THAN 16' FROM THE TRAVELED LANE	
TRAFFIC ENGINEERING		ST CHARLES	

♦ REMOVE OR COVER WHEN NO LONGER APPLICABLE.
 ** REPLACE WITH "ROAD WORK 1/4 MILE" WHEN NO LONGER APPLICABLE.



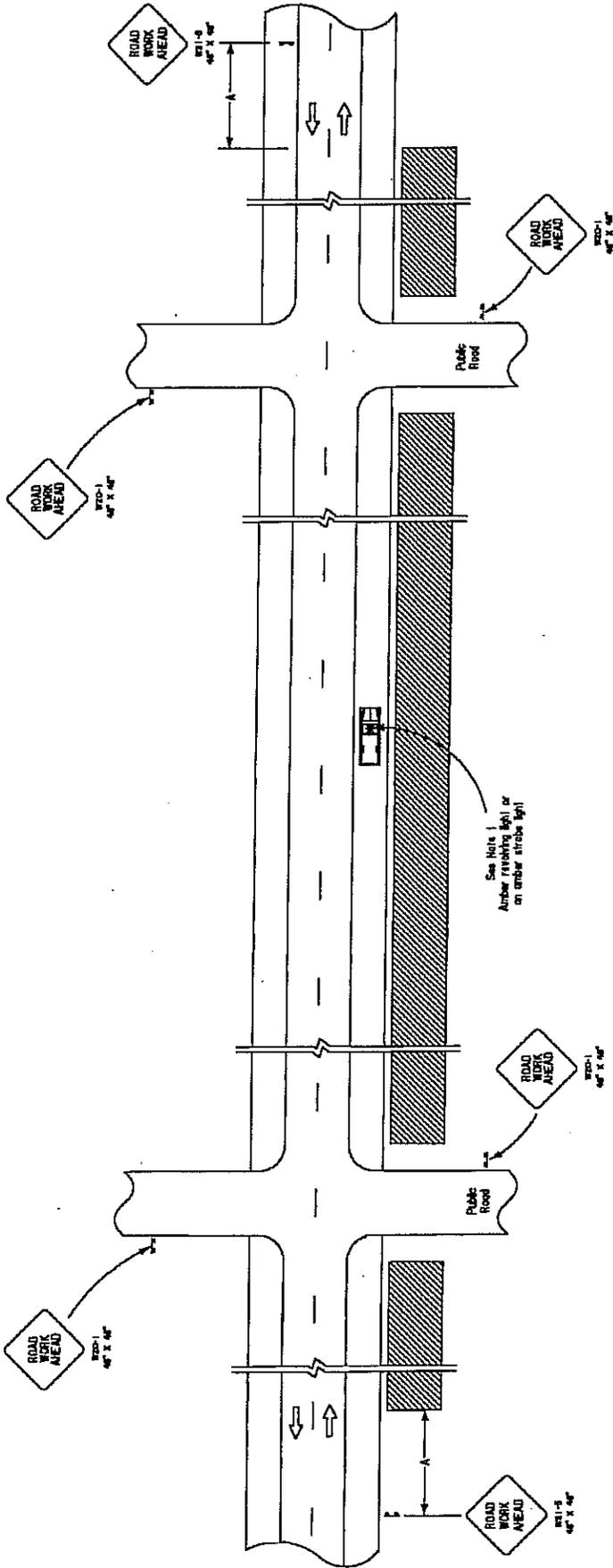
PILOT CAR SHALL BE USED TO GUIDE A QUEUE OF VEHICLES THROUGH THE TEMPORARY TRAFFIC CONTROL DEVICES. THE PILOT CAR SHALL BE USED IN RESTRICTED VISIBILITY OPERATIONS SUCH AS IN CURVY TERRAINS WHERE FLAGGERS CANNOT SEE EACH OTHER (REQUIRE CLEAR LINE-OF-SIGHT). PILOT CARS ARE NOT REQUIRED IN CONFINED OPERATIONS SUCH AS STRIPING OR OTHER LIMITED LANE CLOSURES UNLESS THESE OPERATIONS ARE IN THE VICINITY OF CONTINUOUS LANE CLOSURES. THE OPERATIONS OF THE PILOT VEHICLE SHALL BE COORDINATED WITH FLAGGING OPERATIONS OR OTHER CONTROLS AT EACH END OF THE ONE-LANE SECTION.

SPEED LIMIT (See note 4)	Spacing		
	'A'	'B'	'C'
35 mph	500'	250'	NA
45 mph	1000'	350'	500'
55 mph	1500'	450'	800'



- LEGEND**
- Traffic Sign
 - Flagger
 - Channelizing Device
 - Type III Barricade
 - Work Area
 - Type B Light

- WHEN A WORK AREA HAS BEEN ESTABLISHED ON ONE SIDE OF THE ROADWAY WITHIN 500 FEET OF THE WORK AREA.
- CHANNELIZING DEVICES MAY BE PLACED UP TO 2' BEYOND CENTERLINE ONLY AT SPECIFIC LOCATIONS WHERE ACTUAL WORK ACTIVITY IS TAKING PLACE. A 10' MINIMUM TRAVELED LANE SHOULD BE MAINTAINED WHERE PRACTICAL. CHANNELIZING DEVICES SHALL BE RETURNED TO THE CENTERLINE WHEN THE WORK ACTIVITY HAS PASSED.
- SPACING OF CHANNELIZING DEVICES IN THE TAPER SHOULD BE NO MORE THAN 20'. A MINIMUM OF 3 CHANNELIZING DEVICES ARE TO BE USED IN THE TAPER.
- SPEED LIMIT REFERS TO THE LEGALLY ESTABLISHED SPEED LIMIT BEFORE CONSTRUCTION.
- TO PREVENT VEHICLES FROM ENTERING THE WORK AREA AGAINST THE FLOW OF TRAFFIC, AN ADDITIONAL FLAGGER SHALL BE STATIONED AT EACH INTERSECTION, MAJOR DRIVEWAY, RAILROAD CROSSING OR CROSSING WITHIN THE WORK AREA.
- WITH THE APPROVAL OF THE ENGINEER, THE LENGTH OF THE WORK AREA MAY, FOR A SHORT DURATION, BE CHANGED TO AS MUCH AS ONE-HALF MILE MAXIMUM TO IMPROVE THE SIGHT DISTANCE TO THE FLAGGER. VISUAL OR RADIO CONTACT SHALL BE REQUIRED BETWEEN THE FLAGGERS AT ALL TIMES.
- FOR PROJECTS IN RURAL AREAS THE DISTANCE BETWEEN FLAGGERS SHALL NOT EXCEED 2.5 MILES FOR A.D.T. (AVERAGE DAILY TRAFFIC) OF LESS THAN 200 VEHICLES PER A.D.T. FROM 2,000 TO 5,000. DISTANCE BETWEEN FLAGGERS SHALL NOT EXCEED 1.5 MILES FOR A.D.T. GREATER THAN 5,000 VEHICLES.
- THE CONTRACTOR MAY EXTEND THE LANE CLOSURE AN ADDITIONAL 1.0 MILE UNDER THE FOLLOWING PROVISIONS:
 - THE LANE CLOSURE EXTENSION IS PERMITTED ONLY DURING NON-PEAK HOURS.
 - ONCE THE TRAFFIC CONTROL DEVICES HAVE BEEN PLACED TO EXTEND THE LANE CLOSURE, THE TRAFFIC CONTROL DEVICES AT THE BEGINNING OF THE WORK AREA TO THE DISTANCE DEFINED IN NOTE 7.
- ANY SIGNS IN CONFLICT WITH CONSTRUCTION SIGNING SHALL BE REMOVED OR COVERED.
- MINIMUM CONSTRUCTION SIGNING: ANY ADDITIONAL SIGNS SHOWN IN THIS MANUAL OR TEMPORARY FLAG CAR SIGNING SHALL BE INSTALLED UNDER ITEM 7.15-01, BY THE PROJECT ENGINEER. SIGNING SHALL BE INSTALLED UNDER ITEM 7.15-01.
- VISUAL OR RADIO CONTACT SHALL BE REQUIRED BETWEEN FLAGGERS AT ALL TIMES. THE FLAGGER SHALL BE VISIBLE FROM FLAGGER SIGN.
- TYPE III BARRICADES SHALL BE PLACED IN THE CLOSED LANE AT A 1000' MAXIMUM SPACING. TYPE III BARRICADES SHALL BE PLACED IN THE OPEN LANE AT A 500' MAXIMUM SPACING. UNFILLED HOLES OR HOLES FILLED WITH TEMPORARY MATERIAL, OR WHERE UNCURED CONCRETE EXISTS.
- THE CONTRACTOR CAN USE EITHER A PILOT CAR OR CHANNELIZING DEVICES IN THE TANGENT SECTION. IF A PILOT CAR IS REQUIRED THEN THE CONTRACTOR SHALL NOT BE REQUIRED TO HAVE CHANNELIZING DEVICES IN THE TANGENT SECTION.
- NEITHER WORK ACTIVITY NOR STORAGE OF EQUIPMENT, VEHICLES, OR MATERIALS SHALL OCCUR WITHIN THE BUFFER SPACE.



NOTES

THIS SHEET SHALL BE USED WITH THE "TEMPORARY TRAFFIC CONTROL" GENERAL NOTES SHEET (TC-001).

1. THIS LAYOUT REPRESENTS TRAFFIC CONTROLS REQUIRED FOR WORKERS AND EQUIPMENT OPERATING OUTSIDE OF CLEAR ZONE. IF THE OPERATION RESULTS IN EQUIPMENT OR OTHER VEHICLES BEING PARKED WITHIN THE CLEAR ZONE, BUT NOT WITHIN THE ROADWAY EACH VEHICLE SHALL HAVE AN AMBER LIGHT.
2. WHEN A WORK AREA HAS BEEN ESTABLISHED ON ONE SIDE OF THE ROADWAY ONLY, THERE SHALL BE NO PARKING ON THE OPPOSITE SHOULDER WITHIN 500 FEET OF THE WORK AREA.
3. SPEED LIMIT REFERS TO THE LEGALLY ESTABLISHED SPEED LIMIT BEFORE CONSTRUCTION.
4. AN ADDITIONAL "ROAD WORK AHEAD" SIGN SHALL BE PLACED AT EACH PUBLIC ROAD INTERSECTING THE PROJECT WITHIN THE WORK AREA.
5. ANY SIGNS IN CONFLICT WITH CONSTRUCTION SIGNING SHALL BE REMOVED OR COVERED.
6. MINIMUM CONSTRUCTION SIGNING: ANY ADDITIONAL SIGNS SHOWN IN THIS LAYOUT ARE MINIMUM TRAFFIC CONTROL DEVICES, AND REQUIRED BY THE PROJECT ENGINEER SHALL BE INSTALLED UNDER ITEM 713-01.

LEGEND

- ⊥ Traffic Sign
- ▨ Work Area

SPEED LIMIT	Spacing
35 mph	'A'
45 mph	500'
55 mph	1000'
	1500'

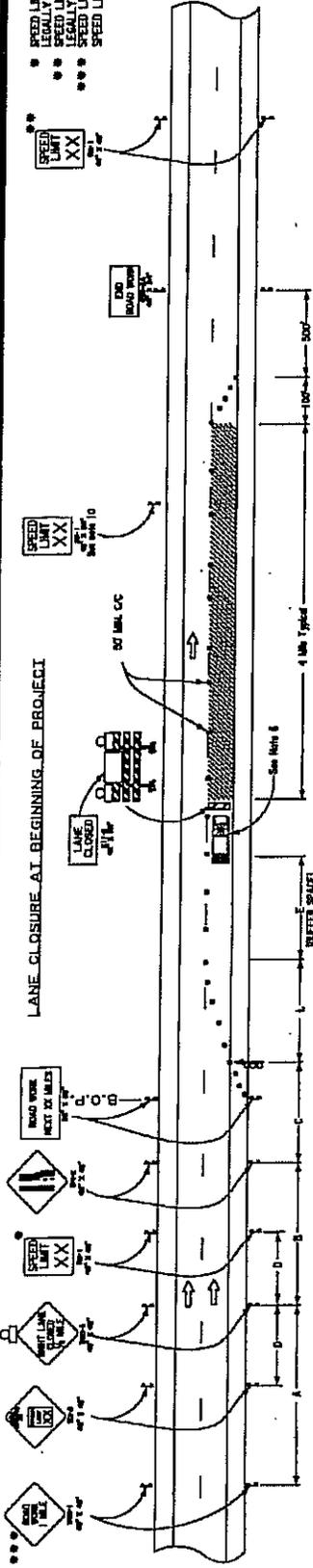


TRAFFIC ENGINEERING		TC-05		TRAFFIC CONTROL FOR WORK GREATER THAN 10' FROM THE TRAVELED LANE				(NO.) DATE BY		REVISION DESCRIPTION	
DESIGNED & CHECKED		DATE		PARISH		STATE PROJECT		SHEET NO.		6d	
DETAILED & CHECKED		DATE		ST CHARLES		007-03-0056					

SPEED LIMIT IS TO BE 10 MPH LESS THAN LEGALLY ESTABLISHED SPEED LIMIT
 ** SPEED LIMIT IS TO BE RETURNED TO LEGALLY ESTABLISHED SPEED LIMIT
 *** SPEED LIMIT IS TO BE RETURNED TO LEGALLY ESTABLISHED SPEED LIMIT
 **** SPEED LIMIT IS TO BE RETURNED TO LEGALLY ESTABLISHED SPEED LIMIT
 ***** SPEED LIMIT IS TO BE RETURNED TO LEGALLY ESTABLISHED SPEED LIMIT

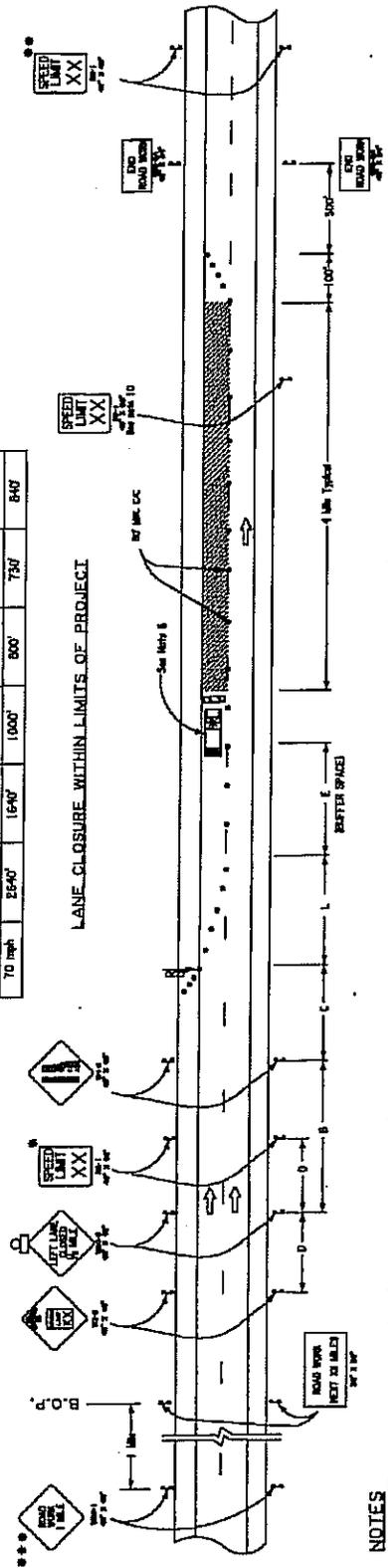
LEGEND

- Traffic Sign
- Channelizing Devices
- Type B Barricades
- Flashing Arrow Panel
- Work Area
- Type B Light



SPEED LIMIT	Spacing					
	A'	B'	C'	T'	E'	V'
45 mph	1140'	1000'	500'	500'	360'	540'
55 mph	2640'	1640'	1000'	800'	495'	660'
60 mph	2640'	1640'	1000'	800'	570'	780'
65 mph	2640'	1640'	1000'	800'	645'	780'
70 mph	2640'	1640'	1000'	800'	730'	840'

LANE CLOSURE WITHIN LIMITS OF PROJECT



NOTES

1. SPEED LIMIT SHALL BE USED WITH THE "TEMPORARY TRAFFIC CONTROL GENERAL NOTES SHEET (TC-001)".
2. WHEN DOING ANY INTERSTATE WORK, A MINIMUM OF TWO DYNAMIC MESSAGE SIGNS PER DIRECTION SHALL BE PLACED IN ADVANCE OF THE LANE CLOSURE. PLACEMENT AS TO PLACEMENT IS SHOWN ON TC-002; HOWEVER, SPECIFIC DISTANCES TO BE SET BY THE PROJECT ENGINEER.
3. DOWNSTREAM TAPERS SHALL BE 100' PER LANE WITH CHANNELIZING DEVICES SPACED AT A SPACING OF 20'.
4. TYPE II BARRICADES SHALL BE PLACED IN THE CLOSED LANE AT A 1000' INTERVAL. TYPE III BARRICADES SHALL BE PLACED IN THE OPEN LANE AT A 1000' INTERVAL. TYPE III BARRICADES SHALL BE PLACED IN THE OPEN LANE AT A MINIMUM OF UNFILLED HOLES OR HOLES FILLED WITH TEMPORARY MATERIAL, OR WHERE UNSURE CONCRETE EXISTS.
5. IF A RAMP ENTRANCE OR EXIT TAPER FALLS WITHIN THE WORK AREA, REFER TO STANDARD ROAD PLANS TC-08 AND TC-09 FOR TRAFFIC CONTROL DETAILS.
6. A VEHICLE WITH A FLASHING AMBER LIGHT AND A TRUCK MOUNTED ATTENUATOR SHALL BE USED IN ADVANCE OF AREAS WHERE WORKERS ARE PRESENT WITHOUT POSITIVE BARRIER PROTECTION.
7. CHANNELIZING DEVICES MAY ENDOACH UP TO 2 FEET FROM CENTERLINE INTO THE OPEN LANE ONLY AT SPECIFIC LOCATIONS WHERE ACTUAL WORK ACTIVITY IS TAKING PLACE. CHANNELIZING DEVICES SHALL BE RETURNED TO THE CLOSED LANE WHEN THE WORK ACTIVITY HAS PASSED. IN NO CASE SHALL THE MAINLINE WIDTH OF THE TRAVEL LANE BE LESS THAN 10'.
8. A FLAGGER SHALL BE USED TO ALERT MOTORISTS WHEN EQUIPMENT OR WORKERS ENDOACH WITHIN 2 FEET OF AN OPEN LANE. THE FLAGGER MUST BE POSTED ADJACENT TO THE OPEN TRAVEL LANE AND IMMEDIATELY UPSTREAM OF EACH OPERATIONAL ENDOACHMENT SHALL BE HELD TO A MINIMUM.
9. WHEN THE LENGTH OF CLOSURE IS GREATER THAN 1 MILE, INSTALL SPEED LIMIT SIGNS AT 1 MILE INTERVALS.
10. IF CONDITIONS RESULT IN A DROPOFF OR RISE, BETWEEN LANE CLOSURE AND TRAVEL LANE, WHICH EXCEEDS 2 INCHES OVERNIGHT, THE CONTRACTOR SHALL PLACE A TEMPORARY EDGE LINE IN THE OPEN LANE, A MINIMUM OF 1 FOOT FROM THE DROPOFF OR RISE. IF THE CONTRACTOR CHOOSES TO USE DRUMS FOR THE CHANNELIZING DEVICES, THE TEMPORARY EDGE LINE MAY BE OMITTED AS LONG AS THE DEVICE SPACING IS 30' OR LESS. IN EITHER CASE, THE CHANNELIZING DEVICES SHALL BE PLACED IN THE CLOSED LANE DURING NONWORKING HOURS.
11. HIGH INTENSITY FLASHING LIGHTS SHALL BE USED TO MARK THE SECOND ADVANCE WARNING SIGN LANE CLOSED 1/4 MILE. LOW INTENSITY FLASHING LIGHTS SHALL BE USED TO MARK ALL OTHER HAZARDS OFF THE TRAVEL WAY. STEADY BURNING LIGHTS SHALL BE USED ON ALL TRAFFIC CONTROL DEVICES THROUGHOUT THE WORKING PERIOD. AFTER THE TAPER ENDS, FIRST LIGHT IN A WARNING TAPER SHALL BE FLASHING.
12. THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A WARNING TAPER SHALL NOT BE GREATER THAN 30'.
13. ANY SIGNS IN CONFLICT WITH CONSTRUCTION SIGNING SHALL BE REMOVED OR COVERED.
14. MINIMUM CONSTRUCTION SIGNING, ANY ADDITIONAL SIGNS SHOWN IN THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" AND REQUIRED BY THE PROJECT ENGINEER SHALL BE INSTALLED UNDER ITEM 713-01.
15. NEITHER WORK ACTIVITY NOR STORAGE OF EQUIPMENT, VEHICLES, OR MATERIALS SHALL OCCUR WITHIN THE BUFFER SPACE.



TRAFFIC CONTROL LAYOUT FOR LANE CLOSURES ON DIVIDED HIGHWAYS

TC-06



TRAFFIC ENGINEERING

DESIGNED	ST CHARLES
CHECKED	PARISH
DETAILED	FEDERAL PROJECT
CHECKED	FEDERAL PROJECT
DATE	STATE PROJECT
BY	007-03-0056
REVISION DESCRIPTION	

**STATE OF LOUISIANA
DEPARTMENT OF TRANSPORTATION AND
DEVELOPMENT**



**CONSTRUCTION PROPOSAL
INFORMATION
FOR**

**STATE PROJECT NO. 007-03-0056
US 61 BARRIER SYSTEM
(LA 50 TO LA 48)
ROUTE US 61
ST. CHARLES PARISH**

**CONTRACT TIME FORM
COST-PLUS-TIME BIDDING PROCEDURE
(A + B) METHOD**

STATE PROJECT NO(S). 007-03-0056

FEDERAL AID PROJECT NO(S). NA

NAME OF PROJECT US 61 BARRIER SYSTEM (LA 50 TO LA 48)

ROUTE US 61

PARISH ST. CHARLES

CONTRACT TIME

The bidder shall determine the number of calendar days required for completion and final acceptance of the project and shall state this required time, in words, in the space provided below. The maximum allowable contract time for this project is **one hundred twenty five (125)** calendar days. The proposed completion time will be a factor used in considering bids for award of contract in accordance with the special provision, COST-PLUS-TIME BIDDING PROCEDURE (A+B METHOD). The stated number of calendar days required for completion will be the contract time for this project should the bidder be successful. Bids not including a contract time, or showing contract time in excess of the maximum allowable amount, will be considered irregular and will be rejected.

CONTRACT TIME (Calendar Days To Completion, In Words)
_____ Calendar Days

BID BOND

A Bid Bond is required when the bidder's total bid amount as calculated by the Department in accordance with Subsection 103.01 is greater than \$50,000. (See Section 102 of the Project Specifications.)

_____, as Principal
(Bidder) and _____, as Surety,
are bound unto the State of Louisiana, Department of Transportation and Development, (hereinafter called the Department) in the sum of five percent (5%) of the bidder's total bid amount as calculated by the Department for payment, of which the Principal and Surety bind themselves, their heirs, executors, administrators, successors and assigns, as solidary obligors.

Signed and sealed this _____ day of _____, 20_____.

The condition of this obligation is such that, whereas the Principal has submitted a bid to the Department on a contract for the construction of **STATE PROJECT NO. 007-03-0056, US 61 BARRIER SYSTEM (LA 50 TO LA 48), located in ST. CHARLES PARISH, ROUTE US 61**, if the bid is accepted and the Principal, within the specified time, enters into the contract in writing and gives bond with Surety acceptable to the Department for payment and performance of said contract, this obligation shall be void; otherwise to remain in effect.

Principal (Bidder or First Partner to Joint Venture)	If a Joint Venture, Second Partner
By _____	By _____
Authorized Officer-Owner-Partner	Authorized Officer-Owner-Partner
_____	_____
Typed or Printed Name	Typed or Printed Name

Surety
By _____ (Seal)
Agent or Attorney-in-Fact

Typed or Printed Name

To receive a copy of the contract and subsequent correspondence / communication from LA DOTD, with respect to the bid bonds, the following information must be provided:

Bonding Agency or Company Name	Address
Agent or Representative	Phone Number / Fax Number



Louisiana Department of Transportation and Development
Proposal Schedule of Items

Contract ID: 007-03-0056

Project(s): 007-03-0056

SECTION: 1

General Items

Proposal Line Number	Item ID	Description Unit Price (In Words, Ink or Typed)	Approximate Quantity	Unit of Measure
0001	202-02-14500	Removal of Guard Rail	380.000	LNFT
				Dollars
				Cents
0002	202-03-38000	Relocation of Sign Various Traffic Warning Signs	34.000	EACH
				Dollars
				Cents
0003	203-07-00100	Borrow (Vehicular Measurement)	500.000	CUYD
				Dollars
				Cents
0004	204-06-00100	Temporary Silt Fencing	30,895.000	LNFT
				Dollars
				Cents
0005	502-01-00200	Superpave Asphaltic Concrete, Drives, Turnouts and Miscellaneous	8,402.800	TON
				Dollars
				Cents
0006	704-01-02000	Guard Rail (Double Thrie Beam) (3'-1 1/2" post spacing)	833.700	LNFT
				Dollars
				Cents
0007	704-03-00100	Blocked Out Guard Rail	29,365.800	LNFT
				Dollars
				Cents
0008	704-06-00100	Guard Rail Anchor Sections (Trailing End)	37.800	LNFT
				Dollars
				Cents



Louisiana Department of Transportation and Development
Proposal Schedule of Items

Contract ID: 007-03-0056

Project(s): 007-03-0056

SECTION: 1

General Items

Proposal Line Number	Item ID	Description Unit Price (In Words, Ink or Typed)	Approximate Quantity	Unit of Measure
0009	704-06-00200	Guard Rail Anchor Sections (Trailing End) (Single Thrie Beam)	6.300	LNFT
				Dollars
				Cents
0010	704-08-00200	Guard Rail Transitions (Double Thrie Beam)	25.000	LNFT
				Dollars
				Cents
0011	704-11-00100	Guard Rail End Treatment (Flared)	5.000	EACH
				Dollars
				Cents
0012	713-01-00100	Temporary Signs and Barricades		LUMP SUM
				Dollars
				Cents
0013	727-01-00100	Mobilization		LUMP SUM
				Dollars
				Cents
0014	729-16-00300	Object Marker Assembly (Type 3)	13.000	EACH
				Dollars
				Cents
0015	740-01-00100	Construction Layout		LUMP SUM
				Dollars
				Cents
0016	NS-700-00240	Special Guard Rail Anchor Section	2.000	EACH
				Dollars
				Cents



Louisiana Department of Transportation and Development
Proposal Schedule of Items

Contract ID: 007-03-0056

Project(s): 007-03-0056

SECTION: 1

General Items

Proposal Line Number	Item ID	Description Unit Price (In Words, Ink or Typed)	Approximate Quantity	Unit of Measure
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0017	NS-729-00002	Guardrail Barrier Marker	1,220.000	EACH
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_____ Dollars
 _____ Cents

Section: 1

Total: _____

Total Bid: _____

CONSTRUCTION PROPOSAL SIGNATURE AND EXECUTION FORM

THIS FORM, THE SCHEDULE OF ITEMS, AND THE PROPOSAL GUARANTY MUST BE COMPLETED AS INDICATED AND SUBMITTED TO THE LOUISIANA DEPARTMENT OF TRANSPORTATION AND DEVELOPMENT (DOTD) TO CONSTITUTE A VALID BID

STATE PROJECT NO. 007-03-0056

FEDERAL AID PROJECT NO. N/A

NAME OF PROJECT US 61 BARRIER SYSTEM (LA 50 TO LA 48)

I (WE) HEREBY CERTIFY THAT I (WE) HAVE CAREFULLY EXAMINED THE PROPOSAL, PLANS AND SPECIFICATIONS, INCLUDING ANY AND ALL ADDENDA, AND THE SITE OF THE ABOVE PROJECT AND AM (ARE) FULLY COGNIZANT OF ALL PROPOSAL DOCUMENTS, THE MASTER COPY OF WHICH IS ON FILE AT DOTD HEADQUARTERS IN BATON ROUGE, LA., AND ALL WORK, MATERIALS AND LABOR REQUIRED THEREIN, AND AGREE TO PERFORM ALL WORK, AND SUPPLY ALL NECESSARY MATERIALS AND LABOR REQUIRED FOR SUCCESSFUL AND TIMELY COMPLETION OF THE ABOVE PROJECT AND TO ACCEPT THE SUMMATION OF THE PRODUCTS OF THE UNIT PRICES BID ON THE SCHEDULE OF ITEMS ATTACHED HERETO AND MADE A PART HEREOF MULTIPLIED BY THE ACTUAL QUANTITY OF UNIT OF MEASURE PERFORMED FOR EACH ITEM, AS AUDITED BY DOTD, AS FULL AND FINAL PAYMENT FOR ALL WORK, LABOR AND MATERIALS NECESSARY TO COMPLETE THE ABOVE PROJECT, SUBJECT TO INCREASE ONLY FOR PLAN CHANGES (CHANGE ORDERS) APPROVED BY THE DOTD CHIEF ENGINEER OR HIS DESIGNEE. THIS BID IS SUBMITTED IN ACCORDANCE WITH THE GENERAL BIDDING REQUIREMENTS IN THE CONSTRUCTION PROPOSAL AND ALL SPECIAL PROVISIONS, PLANS, SUPPLEMENTAL SPECIFICATIONS, AND THE LOUISIANA STANDARD SPECIFICATIONS FOR ROADS AND BRIDGES (2006 EDITION). I (WE) UNDERSTAND THAT THE SUMMATION OF THE PRODUCTS OF THE UNIT PRICES BID ON THE SCHEDULE OF ITEMS MULTIPLIED BY THE ESTIMATED QUANTITY OF UNIT OF MEASURE FOR EACH ITEM, ALONG WITH ANY OTHER FACTORS SPECIFIED TO BE APPLICABLE SUCH AS CONSTRUCTION TIME AND/OR LANE RENTAL, SHALL BE THE BASIS FOR THE COMPARISON OF BIDS. I (WE) UNDERSTAND THAT THE SCHEDULE OF ITEMS MUST CONTAIN UNIT PRICES WRITTEN OUT IN WORDS AND THAT THE SCHEDULE OF ITEMS SUBMITTED AS PART OF THIS BID IS ON THE FORM SUPPLIED BY DOTD IN THE BID PROPOSAL. MY (OUR) PROPOSAL GUARANTY IN THE AMOUNT SPECIFIED FOR THE PROJECT IS ATTACHED HERETO AS EVIDENCE OF MY (OUR) GOOD FAITH TO BE FORFEITED IF THIS BID IS ACCEPTED BY DOTD AND I (WE) FAIL TO COMPLY WITH ANY REQUIREMENT NECESSARY FOR AWARD AND EXECUTION OF THE CONTRACT, AS WELL AS, SIGN AND DELIVER THE CONTRACT AND PAYMENT/PERFORMANCE/RETAINAGE BOND AS REQUIRED IN THE SPECIFICATIONS.

NONCOLLUSION DECLARATION (APPLICABLE TO FEDERAL-AID PROJECTS)

I (WE) DECLARE UNDER PENALTY OF PERJURY UNDER THE LAWS OF THE UNITED STATES AND THE STATE OF LOUISIANA THAT I (WE) HAVE NOT DIRECTLY OR INDIRECTLY, ENTERED INTO ANY AGREEMENT, PARTICIPATED IN ANY COLLUSION, OR OTHERWISE TAKEN ANY ACTION IN RESTRAINT OF FREE COMPETITIVE BIDDING IN CONNECTION WITH THE CONTRACT FOR THIS PROJECT NOR VIOLATED LA. R.S. 48:254.

BIDDER'S DBE GOAL STATEMENT (APPLICABLE TO DBE GOAL PROJECTS)

IF THIS PROJECT IS DESIGNATED BY SPECIAL PROVISION AS A DISADVANTAGED BUSINESS ENTERPRISE (DBE) GOAL PROJECT IN ACCORDANCE WITH THE DBE PROVISIONS OF THIS CONTRACT, THE BIDDER ASSURES DOTD THAT HE/SHE WILL MEET OR EXCEED THE DBE CONTRACT GOAL, OR IF THE BIDDER CANNOT MEET THE REQUIRED DBE GOAL, THE BIDDER ASSURES DOTD THAT HE/SHE HAS MADE AND CAN DOCUMENT GOOD FAITH EFFORTS MADE TOWARDS MEETING THE GOAL REQUIREMENT IN ACCORDANCE WITH THE CONTRACT AND DBE PROGRAM MANUAL INCORPORATED HEREIN BY REFERENCE.

THE APPARENT LOW BIDDER SHALL COMPLETE AND SUBMIT TO THE DOTD COMPLIANCE PROGRAMS OFFICE, FORM CS-6AAA AND ATTACHMENT(S) AND, IF NECESSARY, DOCUMENTATION OF GOOD FAITH EFFORTS MADE BY THE BIDDER TOWARD MEETING THE GOAL, WITHIN TEN BUSINESS DAYS AFTER THE OPENING OF BIDS FOR THIS PROJECT. RESPONSIVENESS OF INFORMATION SUPPLIED IN THIS SECTION OF THIS CONSTRUCTION PROPOSAL SIGNATURE AND EXECUTION FORM IS GOVERNED BY THE DBE REQUIREMENTS INCLUDED WITHIN THE SPECIFICATIONS AND DBE PROGRAM MANUAL.

CERTIFICATION OF EMPLOYMENT OF LOUISIANA RESIDENTS TRANSPORTATION INFRASTRUCTURE MODEL FOR ECONOMIC DEVELOPMENT (TIME) PROJECTS (APPLICABLE TO TIME PROJECTS)

IF THIS PROJECT IS DESIGNATED BY SPECIAL PROVISION AS A TRANSPORTATION INFRASTRUCTURE MODEL FOR ECONOMIC DEVELOPMENT (TIME) PROJECT AS DEFINED IN ACT NO. 16 OF THE 1989 FIRST EXTRAORDINARY SESSION OF THE LEGISLATURE WHICH ENACTED PART V OF CHAPTER 7 OF SUBTITLE II OF TITLE 47 OF THE LOUISIANA REVISED STATUTES OF 1950, COMPRISED OF R.S. 47:820.1 THROUGH 820.6.

THE BIDDER CERTIFIES THAT AT LEAST 80 PERCENT OF THE EMPLOYEES EMPLOYED ON THIS TIME PROJECT WILL BE LOUISIANA RESIDENTS IN ACCORDANCE WITH LOUISIANA R.S. 47:820.3.

NON PARTICIPATION IN PAYMENT ADJUSTMENT (ASPHALT CEMENT AND FUELS) STATEMENT

IF THIS PROJECT IS DESIGNATED BY SPECIAL PROVISION AS BEING SUBJECT TO PAYMENT ADJUSTMENT FOR ASPHALT CEMENT AND/OR FUELS, THE BIDDER HAS THE OPTION OF REQUESTING EXCLUSION FROM SAID PAYMENT ADJUSTMENT PROVISIONS THAT ARE ESTABLISHED BY SPECIAL PROVISION ELSEWHERE HEREIN.

IF THE BIDDER DESIRES TO BE EXCLUDED FROM THESE PAYMENT ADJUSTMENT PROVISIONS,

THE BIDDER IS REQUIRED TO MARK HERE

FAILURE TO MARK THIS BOX PRIOR TO BID OPENING WILL CONSTITUTE FORFEITURE OF THE BIDDER'S OPTION TO REQUEST EXCLUSION.

CS-14A
08/06

BIDDER SIGNATURE REQUIREMENTS (APPLICABLE TO ALL PROJECTS)

THIS BID FOR THE CAPTIONED PROJECT IS SUBMITTED BY:

(Name of Principal (Individual, Firm, Corporation, or Joint Venture))

(If Joint Venture, Name of First Partner)

(Louisiana Contractor's License Number of Bidder or First Partner to Joint Venture)

(Business Street Address)

(Business Mailing Address, if different)

(Area Code and Telephone Number of Business)

(Telephone Number and Name of Contact Person)

(Telecopier Number, if any)

(If Joint Venture, Name of Second Partner)

(Louisiana Contractor's License Number of Second Partner to Joint Venture)

(Business Street Address)

(Business Mailing Address, if different)

(Area Code and Telephone Number of Business)

(Telephone Number and Name of Contact Person)

(Telecopier Number, if any)

ACTING ON BEHALF OF THE BIDDER, THIS IS TO ATTEST THAT THE UNDERSIGNED DULY AUTHORIZED REPRESENTATIVE OF THE ABOVE CAPTIONED FIRM, CORPORATION OR BUSINESS, BY SUBMISSION OF THIS BID, AGREES AND CERTIFIES THE TRUTH AND ACCURACY OF ALL PROVISIONS OF THIS PROPOSAL, INCLUSIVE OF THE REQUIREMENTS, STATEMENTS, DECLARATIONS AND CERTIFICATIONS ABOVE AND IN THE SCHEDULE OF ITEMS AND PROPOSAL GUARANTY. EXECUTION AND SIGNATURE OF THIS FORM AND SUBMISSION OF THE SCHEDULE OF ITEMS AND PROPOSAL GUARANTY SHALL CONSTITUTE AN IRREVOCABLE AND LEGALLY BINDING OFFER BY THE BIDDER.

(Signature)

(Printed Name)

(Title)

(Date of Signature)

(Signature)

(Printed Name)

(Title)

(Date of Signature)

CONTRACTOR'S INFORMATIONAL BID

It is agreed that the total bid shown below, determined by the bidder, is for purposes of opening and reading bids only and that the low bidder for this project will be determined in accordance with the special provision entitled **COST-PLUS-TIME BIDDING PROCEDURE (A+B METHOD)**, as determined by the Department.

A = Summation of products of the quantities shown in the Schedule of Items multiplied by the unit prices.

A = _____

B = Bidders proposed contract time multiplied by the Daily User Cost (\$3000).

B = _____ Calendar Days x \$3000

B = _____

Contractor's Total Bid (A + B) _____