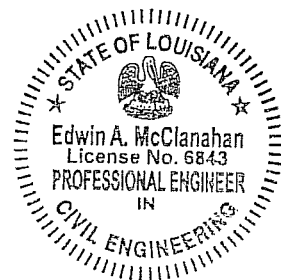


**STATE OF LOUISIANA
DEPARTMENT OF TRANSPORTATION AND
DEVELOPMENT**

CONSTRUCTION PROPOSAL



**STATE PROJECT NO. 151-01-0007 & 840-26-0003
MINOR OVERLAYS NEAR BOYCE (CM)
ROUTES LA 8 & LA 1200
RAPIDES PARISH**



Edwin A. McClanahan
December 11, 2007

FOR INFORMATION ONLY

STATE PROJECT NOS. 151-01-0007 & 840-26-0003
TABLE OF CONTENTS

	Page No.
Title Sheet	A-1
Table of Contents	B-1
Notice to Contractors	C-1 thru C-2
Special Provisions	D-1 thru D-9
Supplemental Specifications:	
Supplemental Specifications for 2006 Standard Specifications (04/07)	E-1 thru E-5
Plans (24 sheets).....	F-1 thru F-24
Construction Proposal Returnables:	
Title Sheet.....	G-1
Bid Bond.....	H-1
Schedule of Items	I-1 thru I-5
Construction Proposal Signature and Execution Form.....	J-1 thru J-2

FOR INFORMATION ONLY

NOTICE TO CONTRACTORS (08/07)

Either sealed paper bids or electronic bids for the following project will be received by the Department of Transportation and Development (DOTD). Paper bids can be delivered to the DOTD Headquarters Administration Building, 1201 Capitol Access Road, Room 405-L, Baton Rouge, Louisiana 70802 until 8:00 a.m. on **Wednesday January 30, 2008**. After 8:00 a.m., paper bids will be received in the Headquarters Auditorium until 10:00 a.m. Electronic bids must be submitted through www.bidx.com prior to the electronic bidding deadline. Beginning at 10:00 a.m., all bids will be publicly opened and presented in the Headquarters Auditorium. No bids will be received after 10:00 a.m. Any person requiring special accommodations shall notify DOTD at (225) 379-1111 not less than 3 business days before bid opening.

STATE PROJECT NOS. 151-01-0007 & 840-26-0003

DESCRIPTION: MINOR OVERLAYS NEAR BOYCE (CM)

ROUTES: LA 8 & LA 1200

PARISH: RAPIDES

LENGTH: 0.086 & 0.731 miles.

TYPE: COLD PLANANING ASPHALTIC CONCRETE, PAVEMENT PATCHING, ASPHALTIC SURFACE TREATMENT, SUPERPAVE ASPHALTIC CONCRETE OVERLAY, AND RELATED WORK

LIMITS: State Project No. 151-01-0007: LOCATED ON ROUTE LA 8 AT ITS JUNCTION WITH MEDFORD STREET IN BOYCE to THE RED RIVER BRIDGE ABUTMENT.

LIMITS: State Project No. 840-26-0003: LOCATED ON ROUTE LA 1200 AT BAYOU BETRAND BRIDGE to WEXFORD STREET IN BOYCE.

ESTIMATED COST RANGE: \$500,000 to \$1,000,000

PROJECT ENGINEER: VOLLMAN, WAYNE, 2918 N. Bolton Avenue, Alexandria, LA 71303, (318) 487-5666

PROJECT MANAGER: MCCLANAHAN, ED, (225) 379-1738

COST OF PROPOSAL FORMS: \$25.00

COST OF PLANS: Included in proposal (no additional charge).

Bids must be submitted in accordance with Section 102 of the 2006 Louisiana Standard Specifications for Roads and Bridges as amended by the project specifications, and must include all information required by the proposal.

FOR INFORMATION ONLY

NOTICE TO CONTRACTORS (CONTINUED)

Plans and/or proposals may be obtained in Room 101-A of the DOTD Headquarters Administration Building, 1201 Capitol Access Road in Baton Rouge, or by contacting the DOTD; Email: sharonknight@dotd.la.gov, Phone (225) 379-1111, FAX: (225) 379-1714, or by written requests sent to the Louisiana Department of Transportation and Development, Project Control Section, P. O. Box 94245, Baton Rouge, LA 70804-9245. Proposals will not be issued later than 24 hours prior to the time set for opening bids. The purchase price for plans and proposals is non-refundable. Plans and specifications may be seen at the Project Engineer's office or in Room 101-A of the DOTD's Headquarters Administration Building in Baton Rouge. Upon request, the Project Engineer will show the work.

The U. S. Department of Transportation (DOT) operates a toll free "Hotline" Monday through Friday, 8:00 a.m. to 5:00 p.m., eastern time. Anyone with knowledge of possible bid rigging, bidder collusion, or other fraudulent activities should call 1-800-424-9071. All information will be treated confidentially and caller anonymity will be respected.

FOR INFORMATION ONLY

STATE PROJECT NOS. 151-01-0007 & 840-26-0003
SPECIAL PROVISIONS

GENERAL BIDDING REQUIREMENTS (08/06): The specifications, contract and bonds governing the construction of the work are the 2006 Edition of the Louisiana Standard Specifications for Roads and Bridges, together with any supplementary specifications and special provisions attached to this proposal.

Bids shall be prepared and submitted in accordance with Section 102 of the Standard Specifications.

The plans herein referred to are the plans approved and marked with the project number, route and Parish, together with all standard or special designs that may be included in such plans. The bidder declares that the only parties interested in this proposal as principals are those named herein; that this proposal is made without collusion or combination of any kind with any other person, firm, association, or corporation, or any member or officer thereof; that careful examination has been made of the site of the proposed work, the plans, Standard Specifications, supplementary specifications and special provisions above mentioned, and the form of contract and payment, performance, and retainage bond; that the bidder agrees, if this proposal is accepted, to provide all necessary machinery, tools, apparatus and other means of construction and will do all work and furnish all material specified in the contract, in the manner and time therein prescribed and in accordance with the requirements therein set forth; and agrees to accept as full compensation therefore, the amount of the summation of the products of the quantities of work and material incorporated in the completed project, as determined by the engineer, multiplied by the respective unit prices herein bid.

It is understood by the bidder that the quantities given in this proposal are a fair approximation of the amount of work to be done and that the sum of the products of the approximate quantities multiplied by the respective unit prices bid shall constitute gross sum bid, which sum shall be used in comparison of bids and awarding of the contract.

The bidder further agrees to perform all extra and force account work that may be required on the basis provided in the specifications.

The bidder further agrees that within 15 calendar days after the contract has been transmitted to him, he will execute the contract and furnish the Department satisfactory surety bonds.

If this proposal is accepted and the bidder fails to execute the contract and furnish bonds as above provided, the proposal guaranty shall become the property of the Department; otherwise, said proposal guaranty will be returned to the bidder; all in accordance with Subsection 103.04.

DEFINITIONS AND TERMS (07/07): Subsection 101.03 of the Standard Specifications is amended as follows.

The definition for "Proposal/ Bid Guaranty" is deleted and following substituted.

Proposal/Bid Guaranty. The required security furnished with a bid. The only form of security acceptable is a Bid Bond.

BIDDING REQUIREMENTS (07/07) Section 102 of the Standard Specifications and the Supplemental Specifications thereto, is amended as follows.

Subsection 102.09, Proposal/Bid Guaranty is deleted and the following substituted.

102.09 PROPOSAL/BID GUARANTY. Each bid shall be accompanied by a proposal/bid guaranty in an amount not less than five percent of the total bid amount when the

FOR INFORMATION ONLY

STATE PROJECT NOS. 151-01-0007 & 840-26-0003
SPECIAL PROVISIONS

bidder's total bid amount as calculated by the Department in accordance with Subsection 103.01 is greater than \$50,000. No proposal/bid guaranty is required for projects when the bidder's total bid amount as calculated by the Department is \$50,000 or less. The official total bid amount for projects that include alternates is the total of the bidder's base bid and all alternates bid on and accepted by the Department. The proposal/bid guaranty submitted by the bidder shall be a bid bond made payable to the contracting agency as specified on the bid bond form provided in the construction proposal. No other form of security will be accepted.

The bid bond shall be on the "Bid Bond" form provided in the construction proposal, on a form that is materially the same in all respects to the "Bid Bond" form provided, or on an electronic form that has received Department approval prior to submission. The bid bond shall be filled in completely, shall be signed by an authorized officer, owner or partner of the bidding entity, or each entity representing a joint venture; shall be signed by the surety's agent or attorney-in-fact; and shall be accompanied by a notarized document granting general power of attorney to the surety's signer. The bid bond shall not contain any provisions that limit the face amount of the bond.

The bid bond will be written by a surety or insurance company that is in good standing and currently licensed to write surety bonds in the State of Louisiana by the Louisiana Department of Insurance and also conform to the requirements of LSA-R.S. 48:253.

All signatures required on the bid bond may be original, mechanical reproductions, facsimiles or electronic. Electronic bonds issued in conjunction with electronic bids must have written Departmental approval prior to use. The Department will make a listing of approved electronic sureties providers on the Bidx.com site.

MAINTENANCE OF TRAFFIC (08/06): Subsection 104.03 of the 2006 Standard Specifications is amended to include the following requirements.

The contractor shall provide for and maintain through and local traffic at all times and shall conduct his operations in such manner as to cause the least possible interference with traffic at junctions with roads, streets and driveways.

Between October 1 and January 31, the contractor shall maintain the highway in a condition suitable for large scale sugar cane hauling operations and prior thereto shall perform only those items which will not interfere with the condition of the highway for heavy hauling operations. During this period, the contractor shall provide all equipment and material necessary to keep the highway in satisfactory condition. If the contractor does not properly maintain the highway, the Department reserves the right to maintain same with its own equipment, labor and material and deduct costs of such maintenance from payments for the work. If it becomes necessary to suspend construction operations for heavy hauling during the sugar cane season, contract time will not be assessed for said period of suspension; however, maintenance of traffic shall be continued by the contractor during such period of suspension.

During asphaltic surface treatment operations, the contractor will be permitted to interrupt traffic for periods not exceeding 50 consecutive minutes in each 60-minute period.

The contractor shall conduct his paving operations on one side of the roadway at a time. The side of the roadway, including shoulder, that is open to traffic shall be clear at all times.

When the plans show asphaltic concrete pavement layers to be placed in thicknesses of 2 inches (50 mm) or less, the contractor will be permitted to pave in one lane for a full day; the adjacent lane may be paved the following workday. When pavement layers are greater than

FOR INFORMATION ONLY

STATE PROJECT NOS. 151-01-0007 & 840-26-0003
SPECIAL PROVISIONS

2 inches (50 mm) thickness, the contractor shall place approximately 1/2 of each day's production in one lane and the remainder in the adjacent lane.

At the end of each day's paving operations, temporary pavement markings shall be in place and proper signs and barricades displayed. During the period that all lanes are open to traffic, the contractor shall neither store material nor park equipment on roadway shoulders.

When asphaltic concrete pavement is cold planed to a depth of 2 inches (50 mm) or less, the contractor will be permitted to cold plane in one lane for a full day; the adjacent lane may be cold planed the following workday. When the depth of cold planing is greater than 2 inches (50 mm), the contractor shall cold plane approximately 1/2 of each day's production in one lane and the remainder in the adjacent lane.

All asphaltic concrete pavement new construction, overlays, and shoulder surfacing operations open to traffic shall be conducted in accordance with the following requirements.

1. Shoulder Subgrade Preparation: Any required embankment widening shall be completed before placement of the asphaltic concrete overlay. All vegetation shall be removed from existing shoulders before beginning temporary or final shoulder construction.

2. Temporary Shoulder Construction: Temporary shoulder construction described herein shall be completed at the end of each day's operations for all asphaltic concrete courses except the final wearing course. There shall be no drop-off from the pavement edge to the shoulder. The contractor shall blade and shape existing shoulder material against, and approximately level with, the top of the pavement surfacing to form a temporary shoulder with a uniform slope from the pavement edge to the existing shoulder line, or to a point 10 feet (3 m) from the pavement edge. If existing shoulder materials are insufficient, the contractor shall furnish, place and shape additional shoulder surfacing materials to form the temporary shoulder. Existing and/or additional materials for temporary shoulders shall be to the satisfaction of the engineer. Compaction shall be by approved methods.

No direct payment will be made for constructing and subsequently reshaping temporary shoulders, except payment for additional materials under appropriate pay items.

ENVIRONMENTAL PROTECTION (08/06): Subsection 107.14 of the 2006 Standard Specifications is amended to include the following paragraphs at the end of this subsection.

The project engineer will complete and submit the Small Construction Activity Completion Report to the LADEQ by January 28th of the year following the calendar year of project acceptance and stabilization.

The use of erosion control features or methods other than those in the contract shall be as directed.

The Storm Water Pollution Prevention Plan shall be comprised of Section 204 of the standard specifications along with applicable supplemental specifications and special provisions, and Standard Plan EC-01, "Temporary Erosion Control Details."

SUBLETTING OF CONTRACT (01/83): In accordance with Subsection 108.01 of the Standard Specifications, the following items are designated as "Specialty Items":

Item 731-02, Reflectorized Raised Pavement Markers

Item 732-01-C, Plastic Pavement Striping (8" Width)

Item 732-01-E, Plastic Pavement Striping (24" Width)

Item 732-02-A, Plastic Pavement Striping (Solid Line) (4" Width)

FOR INFORMATION ONLY

STATE PROJECT NOS. 151-01-0007 & 840-26-0003
SPECIAL PROVISIONS

Item 732-03-A, Plastic Pavement Striping (Broken Line) (4" Width)
Item 732-04-A, Plastic Pavement Legends & Symbols (Arrow)

PAYMENT ADJUSTMENT (03/07): Section 109, Measurement and Payment of the 2006 Standard Specifications and the supplemental specifications thereto, is amended to add the following.

This project is designated for payment adjustment for asphalt cements and fuels in accordance with Subsection 109.09 as follows.

109.09 PAYMENT ADJUSTMENT (ASPHALT CEMENTS AND FUELS).

(a) General: Payment for contract items indicated herein will be adjusted to compensate for cost differentials of Performance Graded (PG) asphalt cements, gasoline, and diesel fuel when such costs increase or decrease more than 5 percent from the Department's established base prices for these items. The base price indices for asphalt cements and fuels will be the monthly price indices in effect at the time bids are opened for the project. The base price indices for asphalt cements will be as stated in paragraph (b) below. The base price index for fuels will be as stated in paragraph (c) below.

Payment adjustments will be made each monthly estimate period when a price index for this period varies more than 5 percent from its respective base price index. The monthly price indices to be used with each monthly estimate will be the price indices for the month in which the estimate period begins.

If the project is placed in default, payment adjustments will be based on the monthly price indices used for the last monthly estimate period prior to the project being placed in default, unless a monthly price index decreases in which case the lower monthly price index will be used.

If it is determined after completion of work on any eligible item that the total quantity paid to date must be adjusted to reflect more accurate quantity determinations, the Department will prorate the additional quantity to be added or subtracted over all previous estimate periods in which the item of work was performed in order to determine additional payment adjustments. If payment adjustments were made during any of these partial estimate periods, this added or subtracted quantity that has been prorated will likewise have payment adjustments calculated and included.

(b) Performance Graded (PG) Asphalt Cements: The base price index will be the monthly price index in effect at the time of bid opening as shown elsewhere herein. The monthly price indices will be the average, excluding the extreme outliers, of the unit prices for PG 64-22, the average, excluding the extreme outliers, of the unit prices for PG 70-22m, and the average, excluding the extreme outliers, of the unit prices for PG 76-22m. The monthly prices for each of these asphalt materials will be F.O.B. refinery or terminal as determined from the quoted prices effective on the first calendar day of each month from suppliers of these materials. Suppliers considered are those who have requested to participate in the liquid asphalt index determination and have supplied materials on DOTD projects within the past twelve months. These suppliers and materials shall be listed on the Department's Qualified Products List (QPL 41) and must be marketed in Louisiana.

Payment adjustments will be made in accordance with the following formulas:

FOR INFORMATION ONLY

STATE PROJECT NOS. 151-01-0007 & 840-26-0003
SPECIAL PROVISIONS

If Monthly Price Index exceeds Base Price Index,
$$P_a = (A - 1.05B) \times C \times D \times (1.00 + T)$$

If Base Price Index exceeds Monthly Price Index,
$$P_a = (0.95B - A) \times C \times D \times (1.00 + T)$$

Where:

- P_a = Price adjustment (increase or decrease) for asphalt cement.
 A = Monthly Price Index for respective PG 64-22, PG 70-22m, or PG 76-22m in dollars per ton/megagram.
 B = Base Price Index for respective PG 64-22, PG 70-22m, or PG 76-22m in dollars per ton/megagram.
 C = Tons/megagrams of asphaltic concrete.
 D = Percent of respective asphalt cement, per job mix formula, in decimals.
 T = Louisiana sales tax percentage, in decimals.
(Note: Local tax is not considered)

The engineer will furnish the weights (mass) of asphaltic concrete placed during the monthly estimate period with the respective asphalt cement content, excluding the asphalt content in reclaimed asphaltic pavement (RAP) as per job mix formula. If the asphalt cement content changes during the estimate period, the respective weight (mass) of asphaltic concrete produced at each cement content will be reported.

Item 510-02, Pavement Widening, and all contract pay items under Sections 502 and 508, will be eligible for payment adjustments of asphalt materials. No payment adjustment will be made for other asphalt materials, including emulsions and cutbacks.

The base price indices for asphalt cements and fuels will be posted on the DOTD internet website before the 10th calendar day of each month at the following URL: www.dotd.louisiana.gov/lettings/lac_price_index/priceindices.asp.

(c) Fuels: The base price index for this project will be the monthly price index in effect when bids are opened for the project. The monthly price index will be the minimum price quotations for unleaded gasoline and No. 2 diesel fuel listed for the New Orleans area in *Platt's Oilgram and Price Report* effective on the first calendar day of each month.

Payment adjustment will be made in accordance with the following formulas:

If Monthly Price Index exceeds Base Price Index,
$$P_a = (A - 1.05B) \times Q \times F$$

If Base Price Index exceeds Monthly Price Index,
$$P_a = (0.95B - A) \times Q \times F$$

FOR INFORMATION ONLY

STATE PROJECT NOS. 151-01-0007 & 840-26-0003
SPECIAL PROVISIONS

Where:

P_a	=	Price adjustment.
A	=	Monthly Price Index in dollars per gallon/liter.
B	=	Base Price Index in dollars per gallon/liter.
Q	=	Pay Item Quantity (Pay Units).
F	=	Fuel Usage Factor Gal (L)/Pay Unit.

The following is a listing of contract pay items that are eligible for payment adjustment and the fuel usage factors that will be used in making such adjustment. Contract items that expand the items listed herein by use of letter designations are also eligible for fuel price adjustments; for example:

Item 601-01-G, Portland Cement Concrete Pavement 8 inches (200 mm) thick.

FOR INFORMATION ONLY

**STATE PROJECT NOS. 151-01-0007 & 840-26-0003
SPECIAL PROVISIONS**

**ELIGIBLE CONTRACT PAY ITEMS & FUEL USAGE FACTORS FOR FUEL
PAYMENT ADJUSTMENT**

ITEM NO.	PAY ITEM	UNITS	MIN. ORIGINAL CONTRACT QUANTITY FOR PAY ADJUSTMENT	FUEL USAGE FACTORS	
				Diesel ²	Gasoline
203-01 ¹	General Excavation	gal/cu yd	10,000 cu yd	0.29	0.15
203-02	Drainage Excavation	gal/cu yd	10,000 cu yd	0.29	0.15
203-03 ¹	Embankment	gal/cu yd	10,000 cu yd	0.29	0.15
203-04	Nonplastic Embankment	gal/cu yd	10,000 cu yd	0.29	0.15
203-07	Borrow (Vehicular Measurement)	gal/cu yd	10,000 cu yd	0.29	0.15
301-01	Class I Base Course	gal/cu yd	3,000 cu yd	0.88	0.57
301-02	Class I Base Course (" Thick)	gal/sq yd	50,000 sq yd	0.04	0.03
302-01	Class II Base Course	gal/cu yd	3,000 cu yd	0.88	0.57
302-02	Class II Base Course (" Thick)	gal/sq yd	50,000 sq yd	0.04	0.03
303-01	In-Place Cement Stabilized Base Course	gal/sq yd	50,000 sq yd	0.04	0.03
304-02	Lime Treatment (Type B)	gal/sq yd	50,000 sq yd	0.04	0.03
304-03	Lime Treatment (Type C)	gal/sq yd	50,000 sq yd	0.04	0.03
304-04	Lime Treatment (Type D)	gal/sq yd	50,000 sq yd	0.04	0.03
305-01	Subgrade Layer (" Thick)	gal/sq yd	50,000 sq yd	0.04	0.03
308-01	In-Place Cement Treated Base Course	gal/sq yd	50,000 sq yd	0.04	0.03
401-01	Aggregate Surface Course (Net Section)	gal/cu yd	3,000 cu yd	0.88	0.57
401-02	Aggregate Surface Course (Adjusted Vehicular Measurement)	gal/cu yd	3,000 cu yd	0.88	0.57
502-01	Superpave Asphaltic Concrete	gal/ton	1000 ton	2.40 ³	0.2
502-02	Superpave Asphaltic Concrete	gal/cu yd	500 cu yd	4.80 ⁴	0.4
502-03	Superpave Asphaltic Concrete (" Thick)	gal/sq yd	10,000 sq yd	0.13 ^{5,6}	0.01 ⁶
508-01	Asphaltic Concrete (SMA)	gal/ton	1000 ton	2.40 ³	0.2
510-02	Pavement Widening	gal/sq yd	3,000 sq yd	0.86	0.24
601-01	Portland Cement Concrete Pavement (" Thick)	gal/sq yd	15,000 sq yd	0.11	0.15

1 If project has both 203-01 & 203-03, only the item with larger quantity is eligible.

2 For fuel adjustment purposes, the term "diesel" shall represent No. 2 or No. 4 fuel oils or any of the liquified petroleum gases, such as propane or butane.

3 If natural gas or coal is used instead of diesel for aggregate drying and heating the fuel usage factor shall be 1.67 gal/ton.

4 If natural gas or coal is used instead of diesel for aggregate drying and heating the fuel usage factor shall be 13.34 gal/cu yd.

5 If natural gas or coal is used instead of diesel for aggregate drying and heating the fuel usage factor shall be 0.09 gal/sq yd.

6 Per inch of thickness.

FOR INFORMATION ONLY

STATE PROJECT NOS. 151-01-0007 & 840-26-0003
SPECIAL PROVISIONS

**ELIGIBLE CONTRACT PAY ITEMS & FUEL USAGE FACTORS FOR FUEL
PAYMENT ADJUSTMENT (METRIC)**

ITEM NO.	PAY ITEM	UNITS	MIN. ORIGINAL CONTRACT QUANTITY FOR PAY ADJUSTMENT	FUEL USAGE FACTORS	
				Diesel ²	Gasoline
203-01 ¹	General Excavation	l/m ³	7,600 m ³	1.44	0.74
203-02	Drainage Excavation	l/m ³	7,600 m ³	1.44	0.74
203-03 ¹	Embankment	l/m ³	7,600 m ³	1.44	0.74
203-04	Nonplastic Embankment	l/m ³	7,600 m ³	1.44	0.74
203-07	Borrow (Vehicular Measurement)	l/m ³	7,600 m ³	1.44	0.74
301-01	Class I Base Course	l/m ³	2,300 m ³	4.36	2.82
301-02	Class I Base Course (mm Thick)	l/m ²	41,800 m ²	0.18	0.14
302-01	Class II Base Course	l/m ³	2,300 m ³	4.36	2.82
302-02	Class II Base Course (mm Thick)	l/m ²	41,800 m ²	0.18	0.14
303-01	In-Place Cement Stabilized Base Course	l/m ²	41,800 m ²	0.18	0.14
304-02	Lime Treatment (Type B)	l/m ²	41,800 m ²	0.18	0.14
304-03	Lime Treatment (Type C)	l/m ²	41,800 m ²	0.18	0.14
304-04	Lime Treatment (Type D)	l/m ²	41,800 m ²	0.18	0.14
305-01	Subgrade Layer (mm Thick)	l/m ²	41,800 m ²	0.18	0.14
308-01	In-Place Cement Stabilized Base Course	l/m ²	41,800 m ²	0.18	0.14
401-01	Aggregate Surface Course (Net Section)	l/m ³	2,300 m ³	4.36	2.82
401-02	Aggregate Surface Course (Adjusted Vehicular Measurement)	l/m ³	2,300 m ³	4.36	2.82
502-01	Superpave Asphaltic Concrete	l/Mg	900 Mg	10.01 ³	0.83
502-02	Superpave Asphaltic Concrete	l/m ³	400 m ³	23.77 ⁴	1.98
502-03	Superpave Asphaltic Concrete (mm Thick)	l/m ²	8,400 m ²	0.59 ^{5,6}	0.45 ⁶
508-01	Asphaltic Concrete (SMA)	l/Mg	900 Mg	10.01 ³	0.83
510-02	Pavement Widening	l/m ²	2,500 m ²	3.89	1.09
601-01	Portland Cement Concrete Pavement (mm Thick)	l/m ²	12,500 m ²	0.5	0.68

- 1 If project has both 203-01 & 203-03, only the item with larger quantity is eligible.
- 2 For fuel adjustment purposes, the term "diesel" shall represent No. 2 or No. 4 fuel oils or any of the liquified petroleum gases, such as propane or butane.
- 3 If natural gas or coal is used instead of diesel for aggregate drying and heating the fuel usage factor shall be 6.97 l/mg.
- 4 If natural gas or coal is used instead of diesel for aggregate drying and heating the fuel usage factor shall be 16.53 l/m³.
- 5 If natural gas or coal is used instead of diesel for aggregate drying and heating the fuel usage factor shall be 0.41 l/m².
- 6 Per mm of thickness.

FOR INFORMATION ONLY

STATE PROJECT NOS. 151-01-0007 & 840-26-0003
SPECIAL PROVISIONS

ITEM S-001, SAW CUTTING: This work item consists of furnishing all equipment, labor, materials and incidentals to perform saw cutting of existing concrete and/or asphalt roadway at the locations as so shown on the plans or as directed by the project engineer.

Saw cutting, completed, will be measured and paid at the contact unit price per linear foot per inch of depth cut.

Payment will be made under:

Item S-001, Saw Cutting, per inch per linear foot.

ITEM S-002, SURFACE PREPARATION (07/04): This item consists of preparing the existing surfaces for single lift overlays.

Surface tolerances on single-lift overlays over existing surfaces shall be in accordance with Section 502 of the Standard Specifications. The contractor has the option of leveling, grinding, cold planing certain areas, or cold planing the entire project in order to meet surface tolerances. The contractor shall not cold plane more than 1/2 inch (13 mm) (average) from the existing surface. The contractor may retain 100 percent of the reclaimed asphaltic pavement (RAP).

Payment will be made under:

Item S-002, Surface Preparation, per lump sum.

CONTRACT TIME (03/05): The entire contract shall be completed in all details and ready for final acceptance in accordance with Subsection 105.17(b) within **THIRTY (30) working days**.

Prior to assessment of contract time, the contractor will be allowed 30 calendar days from the date stipulated in the Notice to Proceed to commence with portions of the contract work including but not limited to assembly periods, preparatory work for materials fabrications such as test piles, or other activities which hinder progress in the beginning stages of construction. Prior to issuance of the Notice to Proceed, the Department will consider extending the assembly period upon written request from the contractor justifying the need for additional time.

The contractor shall be responsible for maintenance of traffic from the beginning of the assembly period. During the assembly period, the contractor will be allowed to do patching and other maintenance work necessary to maintain the roadway with no time charges when approved by the engineer.

If the contractor begins regular construction operations prior to expiration of the assembly period, the assessment of contract time will commence at the time construction operations are begun.

FOR INFORMATION ONLY

LOUISIANA
DEPARTMENT OF TRANSPORTATION AND DEVELOPMENT
SUPPLEMENTAL SPECIFICATIONS
(FOR 2006 STANDARD SPECIFICATIONS)

TABLE OF CONTENTS

SECTION 108 – PROSECUTION AND PROGRESS	
Subsection 108.04 – Prosecution of Work.....	1
SECTION 202 – REMOVING OR RELOCATING STRUCTURES AND OBSTRUCTIONS	
Subsection 202.06 – Plugging or Relocating Existing Water Wells	1
SECTION 302 – CLASS II BASE COURSE	
Subsection 302.05 – Mixing	1
SECTION 502 – SUPERPAVE ASPHALTIC CONCRETE MIXTURES	
Subsection 502.02 – Materials.....	1
SECTION 704 – GUARD RAIL	
Subsection 704.03 – General Construction Requirements	2
SECTION 713 – TEMPORARY TRAFFIC CONTROL	
Subsection 713.06 – Pavement Markings.....	2
SECTION 901 – PORTLAND CEMENT CONCRETE	
Subsection 901.08 – Composition of Concrete.....	3
SECTION 1005 – JOINT MATERIALS FOR PAVEMENTS AND STRUCTURES	
Subsection 1005.04 – Combination Joint Former/Sealer	3
SECTION 1013 – METALS	
Subsection 1013.09 – Steel Piles	4

FOR INFORMATION ONLY

**LOUISIANA
DEPARTMENT OF TRANSPORTATION AND DEVELOPMENT
SUPPLEMENTAL SPECIFICATIONS**

The 2006 Louisiana Standard Specifications for Roads and Bridges and supplemental specifications thereto are amended as follows.

SECTION 108 – PROSECUTION AND PROGRESS:

Subsection 108.04 – Prosecution of Work (03/05) Pages 74 and 75.

Add the following sentence to the third paragraph of Heading (b).

Should the surety or the Department take over prosecution of the work, the contractor shall remain disqualified for a period of one year from the completion of the project, unless debarment proceedings are instituted.

When the Department of Transportation and Development is not the contracting agency on the project, the second paragraph under Heading (c) is deleted.

SECTION 202 – REMOVING OR RELOCATING STRUCTURES AND OBSTRUCTIONS:

Subsection 202.06 – Plugging or Relocating Existing Water Wells (03/04), Page 105.

Delete the first sentence and substitute the following.

All abandoned wells shall be plugged and sealed at the locations shown on the plans, or as directed by the engineer, in accordance with the “Water Well Rules, Regulations, and Standards, State of Louisiana.” This document is available at the Department of Transportation and Development, Water Resources Section, P. O. Box 94245, Baton Rouge, Louisiana 70804-9245. The Water Resource Section’s telephone number is (225) 274-4172.

SECTION 302 – CLASS II BASE COURSE:

Subsection 302.05 – Mixing (08/06), Pages 152 and 153.

Delete the first sentence of Subheading (b)(1), In-Place Mixing, and substitute the following.

In-place mixing shall conform to Heading (a)(1) except that the percentage of Type I portland cement required will be 6 percent by volume.

SECTION 502 – SUPERPAVE ASPHALTIC CONCRETE MIXTURES:

Subsection 502.02 – Materials (08/06), Pages 210 – 213.

Delete Table 502-3, Aggregate Friction Rating under Subheading (c)(1) and substitute the following.

FOR INFORMATION ONLY

Table 502-3
Aggregate Friction Rating

Friction Rating	Allowable Usage
I	All mixtures
II	All mixtures
III	All mixtures, except travel lane wearing courses with plan ADT greater than 7000 ¹
IV	All mixtures, except travel lane wearing courses ²

¹ When plan current average daily traffic (ADT) is greater than 7000, blending of Friction Rating III aggregates and Friction Rating I and/or II aggregates will be allowed for travel lane wearing courses at the following percentages. At least 30 percent by weight (mass) of the total aggregates shall have a Friction Rating of I, or at least 50 percent by weight (mass) of the total aggregate shall have a Friction Rating of II. The frictional aggregates used to obtain the required percentages shall not have more than 10 percent passing the No. 8 (2.36 mm) sieve.

² When the average daily traffic (ADT) is less than 2500, blending of Friction Rating IV aggregates with Friction Rating I and/or II aggregates will be allowed for travel lane wearing courses at the following percentages. At least 50 percent by weight (mass) of the total aggregate in the mixture shall have a Friction Rating of I or II. The frictional aggregates used to obtain the required percentages shall not have more than 10 percent passing the No. 8 (2.36 mm) sieve.

SECTION 704 – GUARD RAIL:

Subsection 704.03 – General Construction Requirements (01/05), Pages 368 and 369.

Add the following to subparagraph (d), Guard Rail End Treatments.

All end treatments shall bear a label indicating the manufacturer and exact product name of the end treatment along with its assigned NCHRP 350 test level. This label shall resist weathering and shall be permanently affixed to the railing in such a way as to be readily visible.

SECTION 713 – TEMPORARY TRAFFIC CONTROL:

Subsection 713.06 – Pavement Markings (08/06), Pages 400 – 403.

Delete Table 713-1, Temporary Pavement Markings and substitute the following.

Table 713-1
Temporary Pavement Markings^{1,2}

		Two-lane Highways	Undivided Multilane Highways	Divided Multilane Highways
S H O R T T E R M	ADT<1500; or ADT>1500 and time<3 days	Lane lines 4-foot (1.2 m) tape on 40-foot (12 m) centers; with "Do Not Pass" and "Pass With Care" signs as required		
	ADT>1500; Time>3 days and<2 weeks	Lane lines 4-foot (1.2-m) tape on 40-foot (12-m) centers with no passing zone markings		
	All ADT's with time <2 weeks		Lane lines 4-foot (1.2m) tape on 40-foot (12 m) centers; double yellow centerline	Lane lines 4-foot (1.2 m) tape on 40-foot (12 m) centers
L O N G T E R M	All ADT's with time >2 weeks	Standard lane lines, no-passing zone markings, legends and symbols and when pavement width is 22 feet (6.7 m) or greater, edge lines	Standard lane lines, centerlines, edge lines, and legends and symbols	Standard lane lines, centerlines, edge lines, and legends and symbols.

¹No-passing zones shall be delineated as indicated whenever a project is open to traffic.

²On all Asphaltic Surface Treatments that are open to traffic and used as a final wearing course or as an interlayer, temporary pavement markings (tabs) on 20-foot (6 m) centers shall be used, in lieu of the 4-foot (1.2 m) tape, on 40-foot (12 m) centers.

SECTION 901 – PORTLAND CEMENT CONCRETE:

Subsection 901.08 – Composition of Concrete (12/05), Pages 732 – 734.

Add the following to Heading (a).

The blended cement containing up to 50 percent of grade 100 or grade 120 ground granulated blast-furnace slag must be in compliance with Subsection 1001.04 for portland blast-furnace slag cement.

SECTION 1005 – JOINT MATERIALS FOR PAVEMENTS AND STRUCTURES:

Subsection 1005.04 – Combination Joint Former/Sealer (11/05), Pages 782 and 783.

Delete Heading (a) and substitute the following.

FOR INFORMATION ONLY

Supplemental Specifications (April 2007)

Page 4 of 4

(a) Description: This joint former/sealer is intended for use in simultaneously forming and sealing a weakened plane in portland cement concrete pavements.

The material shall consist of an elastomeric strip permanently bonded either mechanically or chemically at the top of each of two rigid plastic side frames and covered with a removable plastic top cap. Side frames shall be of such configuration that when the sealer is inserted into plastic concrete and vibrated, a permanent bond forms between side frames and concrete.

Delete Heading (b)(1) and substitute the following.

(1) Elastomer: The elastomer strip portion of the material shall be manufactured from vulcanized elastomeric compound using polymerized chloroprene or thermoplastic vulcanizate as the base polymer, and shall comply with the following requirements:

<u>Property</u>	<u>ASTM Test Method</u>	<u>Requirements</u>	
		<u>Polymerized Chloroprene</u>	<u>Thermoplastic Vulcanizate</u>
Tensile Strength, kPa, Min.	D 412	12,400	7,400
Elongation at Break, % Min.	D 412	200	400
Hardness, Shore A	D 2240	65 ± 10	65 ± 10
Properties after Aging, 70 h @ 100°C	D 573		
Tensile Strength, % Loss, Max.		20	20
Elongation, % loss, Max.		25	25
Hardness, pts. increase, Max.		10	10
Ozone Resistance, 20% strain or bentloop, 300 pphm in air, 70 h @ 40°C	D 1149	no cracks	no cracks
Oil Swell, IRM 903, 70 h @ 100°C, wt change, % Max.	D 471	45	75

Delete Headings (b)(2) and (b)(3) and substitute the following:

(2) Bond of Elastomer to Plastic: The force required to shear the elastomer from the plastic shall be a minimum of 5.0 pounds per linear inch (90 g/mm) of sealer when tested in accordance with DOTD TR 636.

(3) Bond of Plastic to Cement Mortar: This bond will be evaluated and shall meet the following requirements:

The force required to separate the cement mortar from the plastic shall be a minimum of 5.0 pounds per linear inch (90 g/mm) of sealer when tested in accordance with DOTD TR 636.

SECTION 1013 – METALS:

Subsection 1013.09 – Steel Piles (08/06) Page 822.

Delete the title and references to “Steel Piles” in this subsection and substitute “Steel H Piles”.

FOR INFORMATION ONLY

STANDARD PLANS TO BE
USED ON THIS PROJECT

STD. PLN. DATED

DW-04 09-16-82

EC-01 01-14-94

PM-01 01-21-98

STATE OF LOUISIANA
DEPARTMENT OF TRANSPORTATION
AND DEVELOPMENT
PLAN OF PROPOSED

STATE HIGHWAY

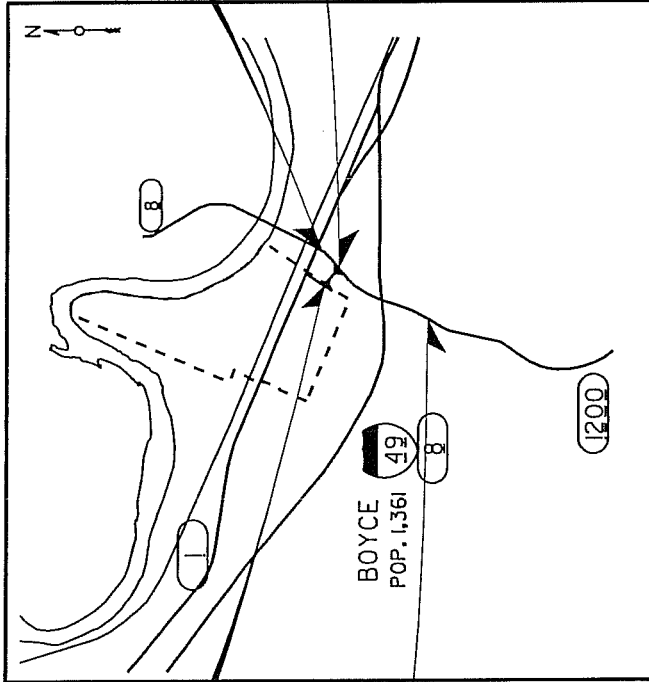
STATE PROJECT NO. 151-01-0007 STATE PROJECT NO. 840-26-0003
LA 8 MINOR OVERLAY (CM) LA 1200 MINOR OVERLAY (CM)
RAPIDES PARISH RAPIDES PARISH
LA 8 LA 1200

C.S. LOG MILE 4.160
STA. 48+62.02
END S. P. No. 840-26-0003

C.S. LOG MILE 3.429
STA. 10+00.00
BEG. S. P. No. 840-26-0003

C.S. LOG MILE 0.115
STA. 14+53.13
END S. P. No. 151-01-0007

C.S. LOG MILE 0.029
STA. 10+00.00
BEG. S. P. No. 151-01-0007



TRAFFIC DATA:
2007 A.D.T. = 3025

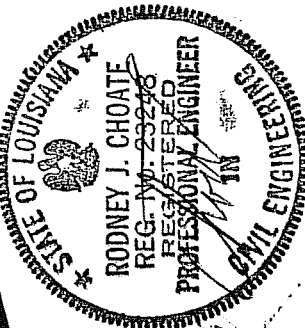
TYPE OF WORK:

PAVEMENT PATCHING, ASPHALTIC SURFACE TREATMENT,
ASPHALTIC CONCRETE WEARING COURSE

The 2006 Louisiana DOTD Standard Specifications for Roads and Bridges,
as amended by the project specifications, shall govern on this project.

Date	Revision	Date	Recommended	Date	Approved

S. P. NO.	PARISH	SHEET NUMBER
151-01-0007 (Lead)	RAPIDES	1



Handwritten signature and date:
11-29-07

Handwritten signature and date:
12/3/07

RECOMMENDED FOR APPROVAL DATE
DISTRICT ADMINISTRATOR

Handwritten signature and date:
12-6-07

APPROVED BY DATE
CHIEF ENGINEER

FOR INFORMATION ONLY

INDEX

STRUCTURE LIST	SHEET NO.	DESCRIPTION	State Project No.	Parish	Sheet No.
			151-01-0007 (Lead)	Rapides	2
No Structures Are Located On This Project	1	Title Sheet			
	2	Index and Structure List			
	3	Design Concept			
	4-6	Project Notes			
	7-8	Typical Sections			
	9	Tabulation: Surfacing (Travelway)(151-01-0007)			
	10	Tabulation: Surfacing (Travelway)(840-26-0003)			
	11	Tabulation: Surfacing (Shoulders)(151-01-0007)			
	12	Tabulation: Surfacing (Shoulders)(840-26-0003)			
	13	Tabulation: Drives and Turnouts			
	14-15	Tabulation: Summary Of Estimated Quantities			
	16	Detail: Paved Drives & Turnouts (Overlay)			
	17	Detail: Butt Joint & Paper Joint			
	18	Detail: TC-00, Temporary Traffic Control (General Notes)			
	19	Detail: TC-01, Temporary Traffic Control (Work Less Than 15' From Travel Lane)			
	20	Detail: TC-02, Temporary Traffic Control (Lane Closure Less Than 1/4 mile In Length)			
	21	Detail: TC-03, Temporary Traffic Control (Lane Closure Greater Than 1/4 mile In Length)			
	22	Detail: TC-05, Temporary Traffic Control (Work Greater Than 15' From The Traveled Way)			
	23	Detail: TC-06, Temporary Traffic Control (Lane Closures, Divided Highway, 60 mph and Over)			
	24	Detail: TC-15, Temporary Traffic Control (Lane Closures for Two Lane Undivided Roads)			

State Project No.	Parish	Sheet No.
151-01-0007 (Lead)	Rapides	3

DESIGN CONCEPT

PROJECT LOCATION AND LIMITS

State Project No. 151-01-0007 (Lead) :

This project begins at station 10+00.00, which is located on La 8, approximately 175.5' northeast of the centerline of Medford Street, and proceeds 0.086 miles to the south Red River bridge abutment, station 14+53.13, where this project ends.

State Project No. 840-26-0003 :

This project begins at station 10+00.00, which is located on La 1200, at the north end of Bayou Bertrand Bridge, and proceeds 0.731 miles to the edge of travel of Wexford Street, station 48+62.02, where this project ends.

EXISTING ROADWAY

State Project No. 151-01-0007 (Lead) :

The existing travelway is a two lane highway with 12' wide lanes. Pavement is asphaltic concrete covered with an asphaltic surface treatment. Existing shoulders are paved.

State Project No. 840-26-0003 :

The existing travelway is a four lane divided highway with 12' wide lanes. Pavement is asphaltic concrete covered with an asphaltic surface treatment. Existing shoulders are paved.

PROPOSED ROADWAY

The roadway shall be patched where directed. The travelway shall receive a two application asphaltic surface treatment interlayer. The travelway shall then be leveled and surfaced with 1.5" of Superpave Asphaltic Concrete Wearing Course. Shoulders shall receive leveling and 2" of Superpave Asphaltic Concrete Wearing Course.

MISCELLANEOUS WORK

Other work shall include plastic pavement markings, and reflectorized raised pavement markers.

State Project No.	Parish	Sheet No.
151-01-0007 (Lead)	Rapides	4

PROJECT NOTES

Sheet 1 of 3	
1)	<p>BUTT JOINTS AND SURFACING AT TIE-INS:</p> <p>Vertical butt joints at tie-ins to existing pavement and structures (where applicable) are required on this project(s) at all locations noted in the plans.</p> <p>The Contractor shall note that such locations may have existing dips or undulations requiring grade adjustments to achieve an acceptable surface.</p> <p>The Engineer will determine if satisfactory results have been achieved by the Contractor and may require from the Contractor, for his own information, profile, string-line and/or survey data of finished work. Unsatisfactory tie-ins shall be corrected by the Contractor, as approved by the Engineer.</p> <p>Butt joints to be paid under Item 509-01.</p>
2)	<p>ITEM 203-07, BORROW (Vehicular Measurement):</p> <p>This item is included for use in that area that exists between the edge of the shoulders and the crowns.</p> <p>Soils shall meet the specifications for their intended usage. All placed borrow shall be compacted as per the Specifications.</p> <p>The Contractor shall not fill in or otherwise obstruct existing ditches or drainage as a result of performing this work.</p>
3)	<p>ITEM 401-02, AGGREGATE SURFACE COURSE (Adjusted Vehicular Measurement):</p> <p>This item is included for use for paved drives and turnouts.</p> <p>This item shall include any grade adjustments to unpaved side roads or driveways necessary for a smooth tie in.</p>
4)	<p>ITEM 507 (Series), ASPHALTIC SURFACE TREATMENT (2 Applications):</p> <p>Treatment shall be Type E, cold applied. Any Portland Cement Concrete drives subject to defacement shall be masked to prevent treatment.</p> <p>The Contractor shall not be foul any maintained lawns with aggregate by brooming or casting material during operations, and shall clean up any spillage as per Section 105.5.</p>
5)	<p>ITEM 509-01, COLD PLANING ASPHALTIC PAVEMENT:</p> <p>ITEM 509-02, CONTRACTOR RETAINED RECLAIMED ASPHALTIC PAVEMENT:</p> <p>This item is included for making butt joints.</p> <p>The Contractor is responsible for all cold planing depth and number of passes required to meet plan intent, with all costs included under this Item.</p> <p>Recycled Asphaltic Pavement (R.A.P.) generated by the project shall be allotted as follows:</p> <p>-- All project generated R.A.P. materials will be retained by the Contractor under the provisions of Contract Item 509-02.</p>

FOR INFORMATION ONLY

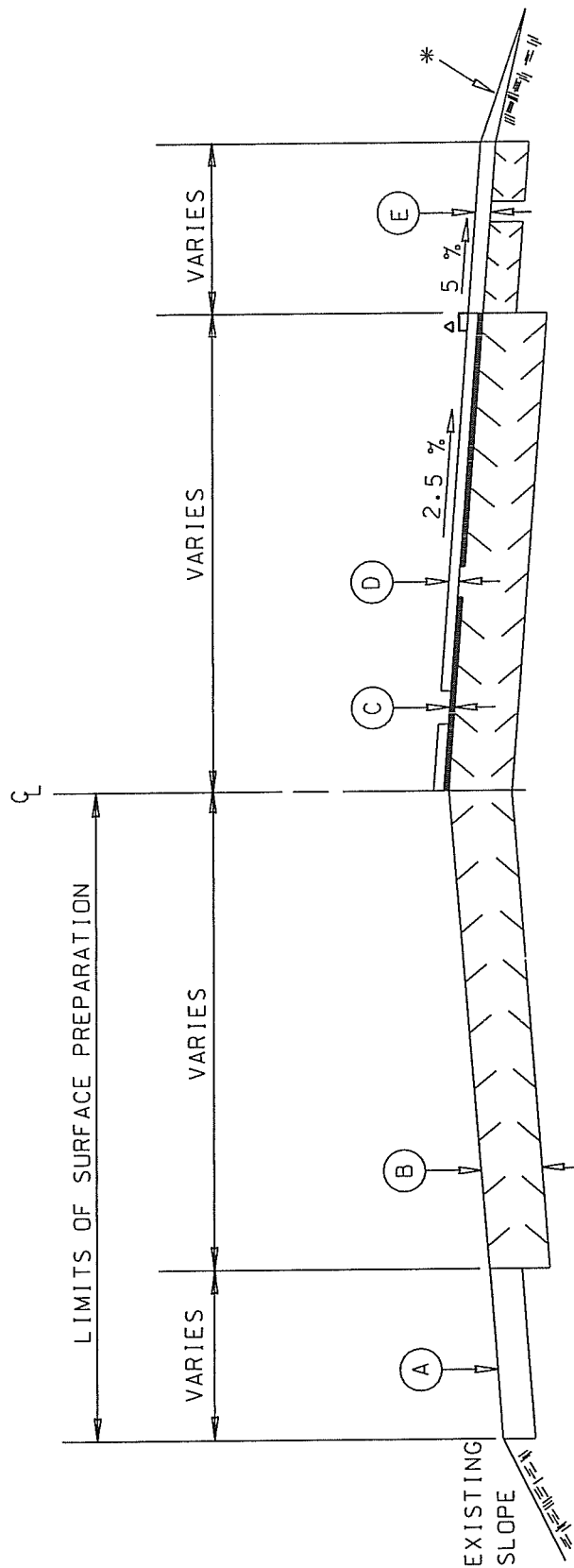
State Project No.	Parish	Sheet No.
151-01-0007 (Lead)	Rapides	5

PROJECT NOTES

Sheet 2 of 3		
6)	ITEM 510-01-A, PAVEMENT PATCHING (6" Minimum Thickness):	
	ITEM 510-01-B, PAVEMENT PATCHING (12" Minimum Thickness):	
	6" Minimum Depth patching is included for use for shoulder patching.	
	12" Minimum Depth patching is included for use for roadway patching.	
	Full depth sawcuts are required around ALL patch perimeters, paid as "Saw Cutting". Unless otherwise allowed or directed, the Contractor shall maintain traffic, and close only one lane of traffic at a time. Unless used in conjunction with Rigid Pavement during composite patching, all travel lanes shall be open for use during non-working hours when prosecuting this work. When patching depth exceeds 12" asphaltic concrete patching material shall be placed in at least two courses of approximately equal thickness. Each course of patch shall receive compactive effort and the patch shall meet the density requirements of the Specifications. Payment adjustment for overdepth or underdepth will be as per Section 510.	
7)	ITEM 713-06-A, TEMPORARY REFLECTORIZED RAISED PAVEMENT MARKERS (Tabs):	
	Plan quantity includes one set of reflective surfaces per surface treatment application.	
8)	ITEM 716-01-A, MULCH (VEGETATIVE):	
	ITEM 717-01, SEEDING:	
	ITEM 718-01, FERTILIZER:	
	The estimated quantities are based on 1.8 acres, at 30 pounds of seed per acre.	
	The estimated quantities for these items are based on expected usage and will be adjusted if deemed appropriate by the Engineer.	
	The Contractor is responsible for timely execution of this work to prevent erosion of soils.	
9)	ITEM 732-01-C, PLASTIC PAVEMENT STRIPING (8" Width):	
	This item is included to replace existing gore area striping and existing turn lane striping.	
10)	ITEM 732-01-E, PLASTIC PAVEMENT STRIPING (24" Width):	
	This item is included to replace existing gore area striping and two stop bars.	
	The Engineer will consult the District Traffic Operations Engineer for the precise placement of the marking.	
11)	ITEM 732-04-A, PLASTIC PAVEMENT LEGENDS AND SYMBOLS (Arrow):	
	This item is included for use to replace existing turn lane arrows.	
	The Engineer will consult the District Traffic Operations Engineer for the precise placement of the marking.	

FOR INFORMATION ONLY

NOT TO SCALE		
STATE PROJECT	PARISH	SHEET NO.
151-01-0007 (Lead)	Rapides	7



TYPICAL HALF SECTION
FOR EXISTING ROADWAY

TYPICAL HALF SECTION
FOR PROPOSED ROADWAY

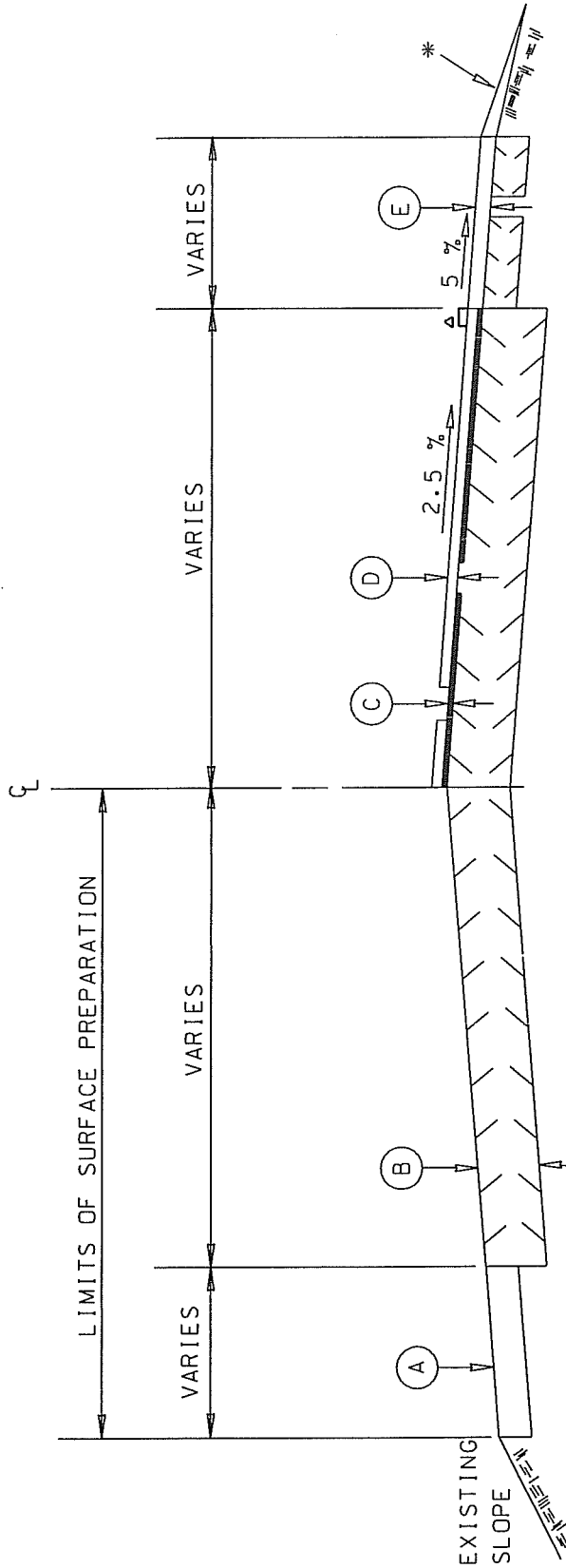
S.P. No. 151-01-0007
APPLIES TO STATIONS

10+00.00 - 14+53.13

- (A) Exist: Paved Shoulders.
- (B) Exist: Base and Surfacing.
- (C) Req: Asph. Surf. Treat. (2 App.). Type E, Cold App.
- (D) Req: 1.5" Superpave Asphaltic Concrete WEARING Course (Level 1)(Plus Leveling and Slope Correction).
- (E) Req: 2" Superpave Asphaltic Concrete WEARING Course (Level A)(Plus Leveling and Slope Correction).
- * Req: Soils To Be Bladed As Directed By Project Engineer.
- Δ Mark for 12' Lanes

20 Year Loading For Mix LevelDesign= 490,800 E.S.A.L.s

NOT TO SCALE	STATE PROJECT	PARISH	SHEET NO.
	151-01-0007 (Lead)	Rapides	8



TYPICAL HALF SECTION
FOR EXISTING ROADWAY

TYPICAL HALF SECTION
FOR PROPOSED ROADWAY

S. P. No. 840-26-0003
APPLIES TO STATIONS

10+00.00 - 48+62.02

- (A) Exist: Paved Shoulders.
- (B) Exist: Base and Surfacing.
- (C) Req: Asph. Surf. Treat. (2 App.), Type E, Cold App.
- (D) Req: 1.5" Superpave Asphaltic Concrete WEARING Course (Level 1)(Plus Leveling and Slope Correction).
- (E) Req: 2" Superpave Asphaltic Concrete WEARING Course (Level A)(Plus Leveling and Slope Correction).
- * Req: Soils To Be Bladed As Directed By Project Engineer.
- ▲ Mark for 12' Lanes

20 Year Loading For Mix LevelDesign= 490,800 E.S.A.L.s

SURFACING (TRAVELWAY)(840-26-0003)

SURFACING (TRAVELWAY)(840-26-0003)				State Project No.		Parish		Sheet	
				151-01-0007 (Lead)		Rapides		10	
								Sheet 1 of 1	
STA.	STA.	DESCRIPTION	Length (ft.)	ITEMS 507 (Series)		ITEM 502-01		1.5 in. (Ton)	
				Asphaltic Surface Treat. Aggregate Width (ft.)	(2 Appl.) (See Note) (sq yd)	Superpave Asphaltic Concrete Width (ft.)			
10+00.00	12+94.00	Roadway Transition	294.00	22.4 Avg.	732	22.4 Avg.	60.4		
12+94.00	15+97.75	Roadway Transition	303.75	26.2 Avg.	884	26.2 Avg.	73.0		
15+97.75	19+13.19	Divided Roadway	315.44	37.7 Avg.	1321	37.7 Avg.	109.0		
19+13.19	23+02.49	Divided Roadway	389.30	46.8 Avg.	2024	46.8 Avg.	167.0		
23+02.49	24+03.69	Roadway	101.20	68.8 Avg.	774	68.8 Avg.	63.8		
24+03.69	31+57.75	Divided Roadway with Turn Lanes	754.06	60.2 Avg.	5044	60.2 Avg.	416.1		
31+57.75	32+60.63	Roadway	102.88	68.2 Avg.	780	68.2 Avg.	64.3		
32+60.63	39+45.28	Divided Roadway	684.65	48.7 Avg.	3705	48.7 Avg.	305.6		
39+45.28	41+78.67	Divided Roadway with Turn Lane	233.39	55.4 Avg.	3213	55.4 Avg.	118.5		
41+78.67	44+16.93	Divided Roadway	238.26	53.0 Avg.	1403	53.0 Avg.	115.8		
44+16.93	45+77.03	Roadway	160.10	25.3 Avg.	450	25.3 Avg.	37.1		
45+77.03	48+62.02	Roadway	284.99	24.5 Avg.	776	24.5 Avg.	64.0		
44+16.93	45+67.55	Ramp A (Left of Centerline)	150.62	58.1 Avg.	972	58.1 Avg.	80.2		
44+16.93	45+49.82	Ramp B (Right of Centerline)	132.89	51.8 Avg.	765	51.8 Avg.	63.1		
47+13.98	48+62.02	Ramp C (Left of Centerline)	148.04	52.1 Avg.	857	52.1 Avg.	70.7		
47+17.00	48+62.02	Ramp D (Right of Centerline)	145.02	71.0 Avg.	1144	71.0 Avg.	94.4		
		Leveling	-	-	-	-	159.5		
			</						

Note : Asphaltic Material Paid Separately Under Item 507-01-A

FOR INFORMATION ONLY

F-10

SUMMARY OF ESTIMATED QUANTITIES									
ITEM NO.	ITEM	UNIT	QUANTITY			S. P. NO.	PARISH	SHEET NO.	
			S.P. NO.	151-01-0007	840-26-0003				
203-07	BORROW (VEHICULAR MEASUREMENT)	CUYD	70				151-01-0007 840-26-0003	14	
401-02	AGGREGATE SURFACE COURSE (ADJUSTED VEHICULAR MEASUREMENT)	CUYD				3			
502-01	SUPERPAVE ASPHALTIC CONCRETE	TON	334.7			3,501.6			3,836.3
502-01-A	SUPERPAVE ASPHALTIC CONCRETE, DRIVES, TURNOUTS AND MISCELLANEOUS	TON				28.0			28.0
507-01-A	ASPHALTIC MATERIAL (CRS-2P EMULSION)	GAL.	989			16,894			17,883
507-02-B	AGGREGATE (S2)	SQYD	1,455			24,844			26,299
507-02-C	AGGREGATE (S3)	SQYD	1,455			24,844			26,299
509-01	COLD PLANING ASPHALTIC PAVEMENT	SQYD	200			647			847
509-02	CONTRACTOR RETAINED RECLAIMED ASPHALTIC PAVEMENT	CUYD	-6			-20			-26
510-01-A	PAVEMENT PATCHING (6" MINIMUM THICKNESS)	SQYD				50			50
510-01-B	PAVEMENT PATCHING (12" MINIMUM THICKNESS)	SQYD				700			700
713-01	TEMPORARY SIGNS & BARRICADES	LUMP	20%			80%			LUMP
713-03-A	TEMPORARY PAVEMENT MARKINGS (BROKEN LINE) (4" WIDTH) (4' LENGTH)	MILE	0.086			1.378			1.464
713-03-B	TEMPORARY PAVEMENT MARKINGS (BROKEN LINE) (4" WIDTH) (10' LENGTH)	MILE				2.589			2.589
713-04-A	TEMPORARY PAVEMENT MARKINGS (SOLID LINE) (4" WIDTH)	MILE	0.343			5.851			6.194
713-06-A	TEMPORARY REFLECTORIZED RAISED PAVEMENT MARKERS (TABS)	EACH	45			728			773
716-01-A	MULCH (VEGETATIVE)	TON	0.4			3.2			3.6
717-01	SEEDING	LB	5			49			54
718-01	FERTILIZER	LB	180			1,624			1,804
727-01	MOBILIZATION	LUMP	10%			90%			LUMP
731-02	REFLECTORIZED RAISED PAVEMENT MARKERS	EACH	147			374			521
732-01-C	PLASTIC PAVEMENT STRIPING (8" WIDTH)	LNFT	942			1,865			2,807
732-01-E	PLASTIC PAVEMENT STRIPING (24" WIDTH)	LNFT	326			415			741
732-02-A	PLASTIC PAVEMENT STRIPING (SOLID LINE) (4" WIDTH)	MILE	0.172			3.039			3.211
732-03-A	PLASTIC PAVEMENT STRIPING (BROKEN LINE) (4" WIDTH)	MILE				1.294			1.294
732-04-A	PLASTIC PAVEMENT LEGENDS & SYMBOLS (ARROW)	EACH				5			5
740-01	CONSTRUCTION LAYOUT	LUMP	10%			90%			LUMP

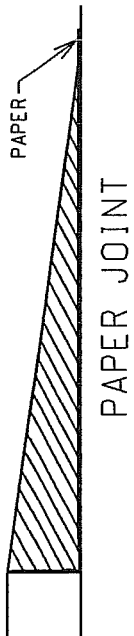
FOR INFORMATION ONLY

DATED 10/17/07 14:23:40

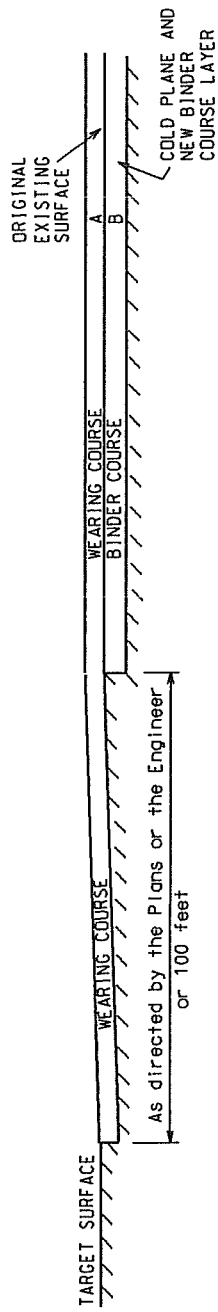
		STATE PROJECT		PARISH		SHEET NO.
		151-01-0007		RAPIDES		
		840-26-0003		RAPIDES		
SUMMARY OF ESTIMATED QUANTITIES						
ITEM NO.	ITEM	UNIT	QUANTITY		TOTAL	
			S.P. NO.	S.P. NO.	QUANTITY	
			151-01-0007	840-26-0003		
S-001	SAW CUTTING	INFT		19,710	19,710	
S-002	SURFACE PREPARATION	LUMP	10%	90%	LUMP	

FOR INFORMATION ONLY
F-15

State Project No.	Parish	Sheet No.
151-01-0007 (Lead)	Rapides	17

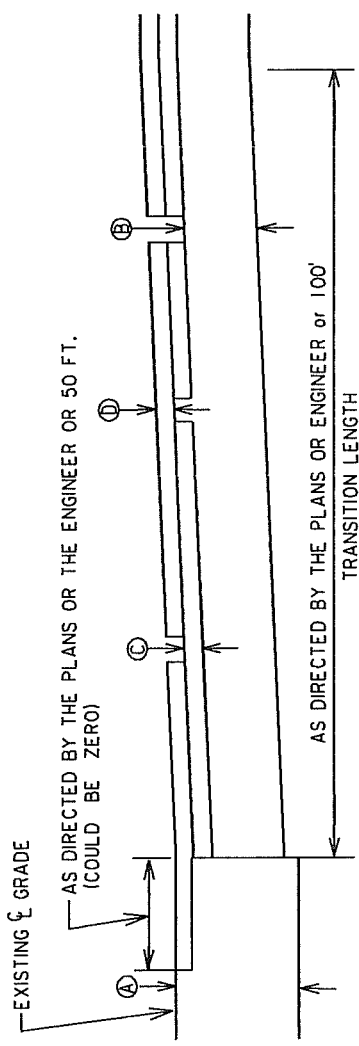


A paper joint shall be used in the construction of all transverse joints in both cold planing, base and asphalt laydown. Paper joint construction will be based on a 1'-0" length per 1/4" of rise. Minimum length shall be 7'-0". Cost of paper joint shall be included in its respective pay item for asphaltic concrete.



LONGITUDINAL COLD PLANING TO BUTT JOINTS, (Typical Case)

(This configuration is typical, other situations will require appropriate cold planing strategies)



- Ⓐ Existing roadway structure
- Ⓑ Required base course (Elevation to be adjusted so that the top of the required wearing course will match grades with existing surfacing).
- Ⓒ Required binder course
- Ⓓ Required wearing course.

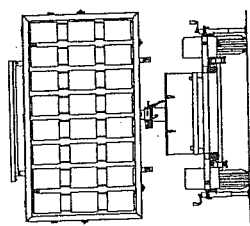
TRANSITION TO BUTT JOINT (NEW BASE)

BUTT JOINT AND PAPER JOINT DETAIL

Not to scale				STATE OF LOUISIANA			
				DEPARTMENT OF TRANSPORTATION AND DEVELOPMENT			
				5-23-06		RJC	
				DATE		BY	
				DESCRIPTION		BY	
				REVISIONS		BY	
						RJC	
						1/1/05	

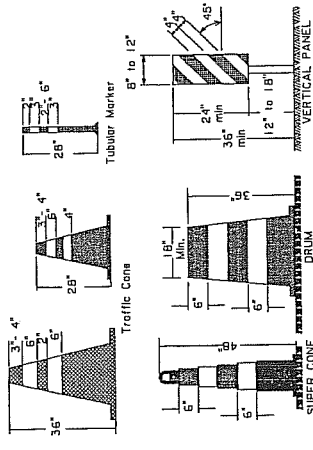
NOTE : USE DETAILS APPLICABLE TO THIS PROJECT

- ## GENERAL PROVISIONS
- All Temporary Traffic Control Devices used shall be in accordance with the L0010 Standard Specifications for Roads and Bridges, the Manual on Uniform Traffic Control Devices (MUTCD), and shall meet the National Cooperative Highway Research Program (NCHRP) 350 for Test Level 3 requirements.
 - Materials used for Temporary Traffic Controls shall be in accordance with the L0010 Standard Specifications for Roads and Bridges and when applicable the L0010 Qualified Products List (QPL).
 - No temporary traffic controls shall be erected without the approval of the Project Engineer and unit work is about to begin, unless they are covered.
 - No lane closures, lane shifts, diversions, or detours shall occur without the authorization of the Project Engineer.
 - Responsibility is hereby placed upon the Contractor for the installation, maintenance, and operation of all temporary traffic control devices called for in these plans or required by the Project Engineer for the protection of the traveling public as well as all Department and construction personnel.
 - The Contractor shall also be responsible for the maintenance of all permanent signs and pavement markings left in place as well as the maintenance of all temporary traffic control devices installed to the safe movement and guidance of traffic within the project limits.
 - The District Traffic Operations Engineer (DTOE) shall serve as the technical advisor to the Project Engineer for all Traffic Control devices.
- Standard Work XX Minus* sign shall be required on all projects and placed at beginning of the project unless otherwise noted. The sign shall be a minimum 36"X60" unless otherwise noted. Warning signs used for lane closures or lane shifts in which the roadway shall be returned to full public use within 14 hours or less shall be placed on NCHRP350 approved portable sign frames.



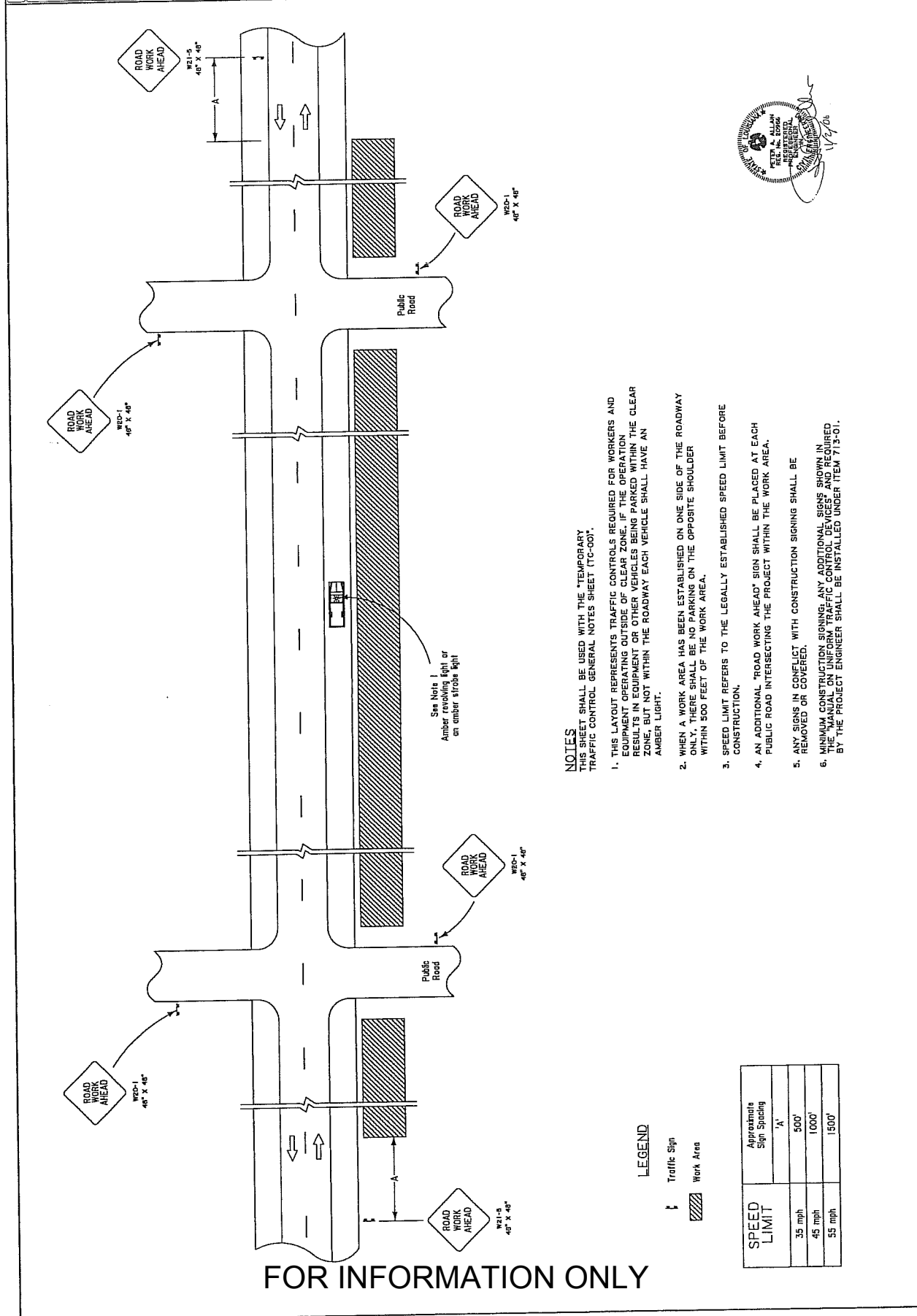
SPEED LIMITS

- Speed limits shall be lowered by 10 mph for any construction, maintenance, or utility operation that requires one or more of the following:
- (A) The condition of the original roadway degraded to the point that it is no longer suitable for travel.
 - (B) Work is being done in the immediate vicinity of the roadway.
 - (C) Any change in lane closures, lane width, lane configuration, or low speed directions (i.e. work areas present in the roadway).
 - (D) The shoulder within 2' of the edge of traveled way is not a barrier condition.
- Only the reduced speed zone shall apply to those portions of the project limits affected. The Project Engineer may allow **SPEED LIMIT WHEN FLASHING** signs to implement reduced speed zones.
- A. At the end of the reduced speed zone, a speed limit sign shall be posted.
 - B. When applying the original speed limit before construction shall be initiated.
- If conditions warrant, the District Traffic Operations Engineer may authorize the reduction of the speed limit by more than 10 mph.



- All pavement markings within the limits of the project that are in conflict with the project signing or the required traffic movements shall be removed from the pavement by blast cleaning or grinding. (Existing striping shall not be painted over with black paint or covered with tape).
- If special pavement markings are needed, they shall be reflectorized, removable, and accompanied by the proper signing.
- Temporary Raised Pavement Markers (TRPMs) may be added to supplement temporary striping in areas of transition, in tapered, in detours, and in other areas of need as directed by the Project Engineer.
- Materials and placement of temporary pavement markings shall conform to section 713 of the Standard Specifications. If no pavement exists, temporary markings will be considered incidental to traffic control.

MUTCD Website:
<http://mutcd.fhwa.dot.gov/>



- NOTES**
THIS SHEET SHALL BE USED WITH THE "TEMPORARY TRAFFIC CONTROL GENERAL NOTES SHEET (TC-00)".
1. THIS LAYOUT REPRESENTS TRAFFIC CONTROLS REQUIRED FOR WORKERS AND EQUIPMENT OPERATING OUTSIDE OF CLEAR ZONE. IF THE OPERATION RESULTS IN EQUIPMENT OR OTHER VEHICLES BEING PARKED WITHIN THE CLEAR ZONE, BUT NOT WITHIN THE ROADWAY EACH VEHICLE SHALL HAVE AN AMBER LIGHT.
 2. WHEN A WORK AREA HAS BEEN ESTABLISHED ON ONE SIDE OF THE ROADWAY ONLY, THERE SHALL BE NO PARKING ON THE OPPOSITE SHOULDER WITHIN 500 FEET OF THE WORK AREA.
 3. SPEED LIMIT REFERS TO THE LEGALLY ESTABLISHED SPEED LIMIT BEFORE CONSTRUCTION.
 4. AN ADDITIONAL "ROAD WORK AHEAD" SIGN SHALL BE PLACED AT EACH PUBLIC ROAD INTERSECTING THE PROJECT WITHIN THE WORK AREA.
 5. ANY SIGNS IN CONFLICT WITH CONSTRUCTION SIGNING SHALL BE REMOVED OR COVERED.
 6. MINIMUM CONSTRUCTION SIGNING, ANY ADDITIONAL SIGNS SHOWN IN THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" AND REQUIRED BY THE PROJECT ENGINEER SHALL BE INSTALLED UNDER ITEM 713-01.

LEGEND

- Traffic Sign
- Work Area

SPEED LIMIT	Approximate Sign Spacing 'A'
35 mph	500'
45 mph	1000'
55 mph	1500'

FOR INFORMATION ONLY

SECTION

1. SPEED LIMIT REFERS TO THE LEGALLY ESTABLISHED SPEED LIMIT BEFORE CONSTRUCTION.
2. WHEN DOING ANY INTERSTATE WORK, A MINIMUM OF TWO DYNAMIC MESSAGE SIGNS PER DIRECTION SHALL BE PLACED IN ADVANCE OF THE LANE CLOSURE, GUIDANCE AS TO PLACEMENT IS IN TC-09; HOWEVER, SPECIFIC DISTANCES TO BE SET BY THE PROJECT ENGINEER.
3. DOWNSTREAM TAPERS SHALL BE 100' PER LANE WITH CHANNELIZING DEVICES PLACED AT A SPACING OF 20'.
4. TYPES (A) BARRICADES SHALL BE PLACED IN THE CLOSED LANE AT A 100' SPACING. WHEN AN ACTIVE WORK IS IN GOING AND THE LANE MUST REMAIN CLOSED, TYPE III BARRICADES ARE ALSO REQUIRED BEFORE EACH OR GROUP OF UNFILLED HOLES OR UNFILLED HOLES FILLED WITH TEMPORARY MATERIAL, ON WHERE UNCURED CONCRETE EXIST.
5. IF A RAMP ENTRANCE OR EXIT TAPER FALLS WITHIN THE WORK AREA, REFER TO STANDARD ROAD PLANS TC-08 AND TC-09 FOR TRAFFIC CONTROL DETAILS.
6. A VEHICLE WITH A FLASHING AMBER LIGHT AND A TRUCK MOUNTED ATTENUATOR MAY BE USED IN ADVANCE OF THE WORK AREA WHERE WORKERS ARE PRESENT WITHOUT POSITIVE BARRIER PROTECTION.

**STATE OF LOUISIANA
DEPARTMENT OF TRANSPORTATION AND
DEVELOPMENT**



**CONSTRUCTION PROPOSAL
RETURNABLES
FOR**

**STATE PROJECT NOS. 151-01-0007 & 840-26-0003
MINOR OVERLAYS NEAR BOYCE (CM)
ROUTES LA 8 & LA 1200
RAPIDES PARISH**

FOR INFORMATION ONLY

BID BOND

A Bid Bond is required when the bidder's total bid amount as calculated by the Department in accordance with Subsection 103.01 is greater than \$50,000. (See Section 102 of the Project Specifications.)

_____, as Principal
(Bidder) _____ and
_____, as Surety,
are bound unto the State of Louisiana, Department of Transportation and Development, (hereinafter called the Department) in the sum of five percent (5%) of the bidder's total bid amount as calculated by the Department for payment, of which the Principal and Surety bind themselves, their heirs, executors, administrators, successors and assigns, as solidary obligors.

Signed and sealed this _____ day of _____, 20_____.

The condition of this obligation is such that, whereas the Principal has submitted a bid to the Department on a contract for the construction of **STATE PROJECT NOS. 151-01-0007 & 840-26-0003, MINOR OVERLAYS NEAR BOYCE (CM), located in RAPIDES PARISH on ROUTES LA 8 & LA 1200**, if the bid is accepted and the Principal, within the specified time, enters into the contract in writing and gives bond with Surety acceptable to the Department for payment and performance of said contract, this obligation shall be void; otherwise to remain in effect.

Principal (Bidder or First Partner to Joint Venture)

By _____

Authorized Officer-Owner-Partner

Typed or Printed Name

If a Joint Venture, Second Partner

By _____

Authorized Officer-Owner-Partner

Typed or Printed Name

Surety

By _____

(Seal)

Agent or Attorney-in-Fact

Typed or Printed Name

To receive a copy of the contract and subsequent correspondence / communication from LA DOTD, with respect to the bid bonds, the following information must be provided:

Bonding Agency or Company Name

Address

Agent or Representative

Phone Number / Fax Number

07/07
Form CS-2A
FOR INFORMATION ONLY

LOUISIANA DEPARTMENT OF TRANSPORTATION AND DEVELOPMENT
SCHEDULE OF ITEMS

DATE: 12/06/07 14:05 PAGE: 1

LEAD PROJECT: 151-01-0007
OTHER PROJECTS: 840-26-0003

ITEM NUMBER	APPROXIMATE QUANTITY	UNIT OF MEASURE	PAY ITEM UNIT PRICE (IN WORDS, INK OR TYPED)
203-07	700	CUBIC YARD	BORROW (VEHICULAR MEASUREMENT) DOLLARS CENTS
401-02	3	CUBIC YARD	AGGREGATE SURFACE COURSE (ADJUSTED VEHICULAR MEASUREMENT) DOLLARS CENTS
501-01	3,836.3	TON	SUPERPAVE ASPHALTIC CONCRETE DOLLARS CENTS
502-01-A	28.0	TON	SUPERPAVE ASPHALTIC CONCRETE, DRIVES, TURNOUTS AND MISCELLANEOUS DOLLARS CENTS
507-01-A	17,883	GALLON	ASPHALTIC MATERIAL (CRS-2P EMULSION) DOLLARS CENTS
507-02-B	26,299	SQUARE YARD	AGGREGATE (S2) DOLLARS CENTS

FOR INFORMATION ONLY

LOUISIANA DEPARTMENT OF TRANSPORTATION AND DEVELOPMENT
SCHEDULE OF ITEMS

DATE: 12/06/07 14:05 PAGE: 2

LEAD PROJECT: 151-01-0007
OTHER PROJECTS: 840-26-0003

ITEM NUMBER	APPROXIMATE QUANTITY	UNIT OF MEASURE	PAY ITEM UNIT PRICE (IN WORDS, INK OR TYPED)
507-02-C	26,299	SQUARE YARD	AGGREGATE (S3) DOLLARS CENTS
508-01	847	SQUARE YARD	COLD PLANING ASPHALTIC PAVEMENT DOLLARS CENTS
508-02	-26	CUBIC YARD	CONTRACTOR RETAINED RECLAIMED ASPHALTIC PAVEMENT DOLLARS CENTS
510-01-A	50	SQUARE YARD	PAVEMENT PATCHING (6" MINIMUM THICKNESS) DOLLARS CENTS
510-01-B	700	SQUARE YARD	PAVEMENT PATCHING (12" MINIMUM THICKNESS) DOLLARS CENTS
713-01	LUMP	LUMP SUM	TEMPORARY SIGNS & BARRICADES DOLLARS CENTS

FOR INFORMATION ONLY

LOUISIANA DEPARTMENT OF TRANSPORTATION AND DEVELOPMENT
SCHEDULE OF ITEMS

DATE: 12/06/07 14:05 PAGE: 3

LEAD PROJECT: 151-01-0007
OTHER PROJECTS: 840-26-0003

ITEM NUMBER	APPROXIMATE QUANTITY	UNIT OF MEASURE	PAY ITEM UNIT PRICE (IN WORDS, INK OR TYPED)
713-03-A	1.464	MILE	TEMPORARY PAVEMENT MARKINGS (BROKEN LINE) (4" WIDTH) (4' LENGTH) DOLLARS CENTS
713-03-B	2.589	MILE	TEMPORARY PAVEMENT MARKINGS (BROKEN LINE) (4" WIDTH) (10' LENGTH) DOLLARS CENTS
713-04-A	6.194	MILE	TEMPORARY PAVEMENT MARKINGS (SOLID LINE) (4" WIDTH) DOLLARS CENTS
714-06-A	773	EACH	TEMPORARY REFLECTORIZED RAISED PAVEMENT MARKERS (TABS) DOLLARS CENTS
716-01-A	3.6	TON	MULCH (VEGETATIVE) DOLLARS CENTS
717-01	54	POUND	SEEDING DOLLARS CENTS

FOR INFORMATION ONLY

LOUISIANA DEPARTMENT OF TRANSPORTATION AND DEVELOPMENT
SCHEDULE OF ITEMS

DATE: 12/06/07 14:05 PAGE: 4

LEAD PROJECT: 151-01-0007
OTHER PROJECTS: 840-26-0003

ITEM NUMBER	APPROXIMATE QUANTITY	UNIT OF MEASURE	PAY ITEM UNIT PRICE (IN WORDS, INK OR TYPED)
718-01	1,804	POUND	FERTILIZER DOLLARS CENTS
727-01	LUMP	LUMP SUM	MOBILIZATION DOLLARS CENTS
732-02	521	EACH	REFLECTORIZED RAISED PAVEMENT MARKERS DOLLARS CENTS
732-01-C	2,807	LINEAR FOOT	PLASTIC PAVEMENT STRIPING (8" WIDTH) DOLLARS CENTS
732-01-E	741	LINEAR FOOT	PLASTIC PAVEMENT STRIPING (24" WIDTH) DOLLARS CENTS
732-02-A	3.211 MILE		PLASTIC PAVEMENT STRIPING (SOLID LINE) (4" WIDTH) DOLLARS CENTS

FOR INFORMATION ONLY

LOUISIANA DEPARTMENT OF TRANSPORTATION AND DEVELOPMENT
SCHEDULE OF ITEMS

DATE: 12/06/07 14:05 PAGE: 5

LEAD PROJECT: 151-01-0007
OTHER PROJECTS: 840-26-0003

ITEM NUMBER	APPROXIMATE QUANTITY	UNIT OF MEASURE	PAY ITEM UNIT PRICE (IN WORDS, INK OR TYPED)
732-03-A	1.294	MILE	PLASTIC PAVEMENT STRIPING (BROKEN LINE) (4" WIDTH) DOLLARS CENTS
732-04-A	5	EACH	PLASTIC PAVEMENT LEGENDS & SYMBOLS (ARROW) DOLLARS CENTS
740-01	LUMP	LUMP SUM	CONSTRUCTION LAYOUT DOLLARS CENTS
S-001	19,710	INCH FOOT	SAW CUTTING DOLLARS CENTS
S-002	LUMP	LUMP SUM	SURFACE PREPARATION DOLLARS CENTS

FOR INFORMATION ONLY

CONSTRUCTION PROPOSAL SIGNATURE AND EXECUTION FORM

THIS FORM, THE SCHEDULE OF ITEMS, AND THE PROPOSAL GUARANTY MUST BE COMPLETED AS INDICATED AND SUBMITTED TO THE LOUISIANA DEPARTMENT OF TRANSPORTATION AND DEVELOPMENT (DOTD) TO CONSTITUTE A VALID BID

STATE PROJECT NO(S). 151-01-0007 & 840-26-0003

FEDERAL AID PROJECT NO(S). N/A

NAME OF PROJECT MINOR OVERLAYS NEAR BOYCE (CM)

I (WE) HEREBY CERTIFY THAT I (WE) HAVE CAREFULLY EXAMINED THE PROPOSAL, PLANS AND SPECIFICATIONS, INCLUDING ANY AND ALL ADDENDA, AND THE SITE OF THE ABOVE PROJECT AND AM (ARE) FULLY COGNIZANT OF ALL PROPOSAL DOCUMENTS, THE MASTER COPY OF WHICH IS ON FILE AT DOTD HEADQUARTERS IN BATON ROUGE, LA., AND ALL WORK, MATERIALS AND LABOR REQUIRED THEREIN, AND AGREE TO PERFORM ALL WORK, AND SUPPLY ALL NECESSARY MATERIALS AND LABOR REQUIRED FOR SUCCESSFUL AND TIMELY COMPLETION OF THE ABOVE PROJECT AND TO ACCEPT THE SUMMATION OF THE PRODUCTS OF THE UNIT PRICES BID ON THE SCHEDULE OF ITEMS ATTACHED HERETO AND MADE A PART HEREOF MULTIPLIED BY THE ACTUAL QUANTITY OF UNIT OF MEASURE PERFORMED FOR EACH ITEM, AS AUDITED BY DOTD, AS FULL AND FINAL PAYMENT FOR ALL WORK, LABOR AND MATERIALS NECESSARY TO COMPLETE THE ABOVE PROJECT, SUBJECT TO INCREASE ONLY FOR PLAN CHANGES (CHANGE ORDERS) APPROVED BY THE DOTD CHIEF ENGINEER OR HIS DESIGNEE. THIS BID IS SUBMITTED IN ACCORDANCE WITH THE GENERAL BIDDING REQUIREMENTS IN THE CONSTRUCTION PROPOSAL AND ALL SPECIAL PROVISIONS, PLANS, SUPPLEMENTAL SPECIFICATIONS, AND THE LOUISIANA STANDARD SPECIFICATIONS FOR ROADS AND BRIDGES (2006 EDITION). I (WE) UNDERSTAND THAT THE SUMMATION OF THE PRODUCTS OF THE UNIT PRICES BID ON THE SCHEDULE OF ITEMS MULTIPLIED BY THE ESTIMATED QUANTITY OF UNIT OF MEASURE FOR EACH ITEM, ALONG WITH ANY OTHER FACTORS SPECIFIED TO BE APPLICABLE SUCH AS CONSTRUCTION TIME AND/OR LANE RENTAL, SHALL BE THE BASIS FOR THE COMPARISON OF BIDS. I (WE) UNDERSTAND THAT THE SCHEDULE OF ITEMS MUST CONTAIN UNIT PRICES WRITTEN OUT IN WORDS AND THAT THE SCHEDULE OF ITEMS SUBMITTED AS PART OF THIS BID IS ON THE FORM SUPPLIED BY DOTD IN THE BID PROPOSAL. MY (OUR) PROPOSAL GUARANTY IN THE AMOUNT SPECIFIED FOR THE PROJECT IS ATTACHED HERETO AS EVIDENCE OF MY (OUR) GOOD FAITH TO BE FORFEITED IF THIS BID IS ACCEPTED BY DOTD AND I (WE) FAIL TO COMPLY WITH ANY REQUIREMENT NECESSARY FOR AWARD AND EXECUTION OF THE CONTRACT, AS WELL AS, SIGN AND DELIVER THE CONTRACT AND PAYMENT/PERFORMANCE/RETAINAGE BOND AS REQUIRED IN THE SPECIFICATIONS.

NONCOLLUSION DECLARATION (APPLICABLE TO FEDERAL-AID PROJECTS)

I (WE) DECLARE UNDER PENALTY OF PERJURY UNDER THE LAWS OF THE UNITED STATES AND THE STATE OF LOUISIANA THAT I (WE) HAVE NOT DIRECTLY OR INDIRECTLY, ENTERED INTO ANY AGREEMENT, PARTICIPATED IN ANY COLLUSION, OR OTHERWISE TAKEN ANY ACTION IN RESTRAINT OF FREE COMPETITIVE BIDDING IN CONNECTION WITH THE CONTRACT FOR THIS PROJECT NOR VIOLATED LA. R.S. 48:254.

BIDDER'S DBE GOAL STATEMENT (APPLICABLE TO DBE GOAL PROJECTS)

IF THIS PROJECT IS DESIGNATED BY SPECIAL PROVISION AS A DISADVANTAGED BUSINESS ENTERPRISE (DBE) GOAL PROJECT IN ACCORDANCE WITH THE DBE PROVISIONS OF THIS CONTRACT, THE BIDDER ASSURES DOTD THAT HE/SHE WILL MEET OR EXCEED THE DBE CONTRACT GOAL, OR IF THE BIDDER CANNOT MEET THE REQUIRED DBE GOAL, THE BIDDER ASSURES DOTD THAT HE/SHE HAS MADE AND CAN DOCUMENT GOOD FAITH EFFORTS MADE TOWARDS MEETING THE GOAL REQUIREMENT IN ACCORDANCE WITH THE CONTRACT AND DBE PROGRAM MANUAL INCORPORATED HEREIN BY REFERENCE.

THE APPARENT LOW BIDDER SHALL COMPLETE AND SUBMIT TO THE DOTD COMPLIANCE PROGRAMS OFFICE, FORM CS-6AAA AND ATTACHMENT(S) AND, IF NECESSARY, DOCUMENTATION OF GOOD FAITH EFFORTS MADE BY THE BIDDER TOWARD MEETING THE GOAL, WITHIN TEN BUSINESS DAYS AFTER THE OPENING OF BIDS FOR THIS PROJECT. RESPONSIVENESS OF INFORMATION SUPPLIED IN THIS SECTION OF THIS CONSTRUCTION PROPOSAL SIGNATURE AND EXECUTION FORM IS GOVERNED BY THE DBE REQUIREMENTS INCLUDED WITHIN THE SPECIFICATIONS AND DBE PROGRAM MANUAL.

CERTIFICATION OF EMPLOYMENT OF LOUISIANA RESIDENTS TRANSPORTATION INFRASTRUCTURE MODEL FOR ECONOMIC DEVELOPMENT (TIME) PROJECTS (APPLICABLE TO TIME PROJECTS)

IF THIS PROJECT IS DESIGNATED BY SPECIAL PROVISION AS A TRANSPORTATION INFRASTRUCTURE MODEL FOR ECONOMIC DEVELOPMENT (TIME) PROJECT AS DEFINED IN ACT NO. 16 OF THE 1989 FIRST EXTRAORDINARY SESSION OF THE LEGISLATURE WHICH ENACTED PART V OF CHAPTER 7 OF SUBTITLE II OF TITLE 47 OF THE LOUISIANA REVISED STATUTES OF 1950, COMPRISED OF R.S. 47:820.1 THROUGH 820.6.

THE BIDDER CERTIFIES THAT AT LEAST 80 PERCENT OF THE EMPLOYEES EMPLOYED ON THIS TIME PROJECT WILL BE LOUISIANA RESIDENTS IN ACCORDANCE WITH LOUISIANA R.S. 47:820.3.

NON PARTICIPATION IN PAYMENT ADJUSTMENT (ASPHALT CEMENT AND FUELS) STATEMENT

IF THIS PROJECT IS DESIGNATED BY SPECIAL PROVISION AS BEING SUBJECT TO PAYMENT ADJUSTMENT FOR ASPHALT CEMENT AND/OR FUELS, THE BIDDER HAS THE OPTION OF REQUESTING EXCLUSION FROM SAID PAYMENT ADJUSTMENT PROVISIONS THAT ARE ESTABLISHED BY SPECIAL PROVISION ELSEWHERE HEREIN.

IF THE BIDDER DESIRES TO BE EXCLUDED FROM THESE PAYMENT ADJUSTMENT PROVISIONS.

THE BIDDER IS REQUIRED TO MARK HERE ☐

FAILURE TO MARK THIS BOX PRIOR TO BID OPENING WILL CONSTITUTE FORFEITURE OF THE BIDDER'S OPTION TO REQUEST EXCLUSION.

FOR INFORMATION ONLY

08/06

J-1

STATE PROJECT NO(S). 151-01-0007 & 840-26-0003

BIDDER SIGNATURE REQUIREMENTS (APPLICABLE TO ALL PROJECTS)

THIS BID FOR THE CAPTIONED PROJECT IS SUBMITTED BY:

(Name of Principal (Individual, Firm, Corporation, or Joint Venture))

(If Joint Venture, Name of First Partner)

(Louisiana Contractor's License Number of Bidder or First Partner to Joint Venture)

(Business Street Address)

(Business Mailing Address, if different)

(Area Code and Telephone Number of Business)

(Telephone Number and Name of Contact Person)

(Telecopier Number, if any)

(If Joint Venture, Name of Second Partner)

(Louisiana Contractor's License Number of Second Partner to Joint Venture)

(Business Street Address)

(Business Mailing Address, if different)

(Area Code and Telephone Number of Business)

(Telephone Number and Name of Contact Person)

(Telecopier Number, if any)

ACTING ON BEHALF OF THE BIDDER, THIS IS TO ATTEST THAT THE UNDERSIGNED DULY AUTHORIZED REPRESENTATIVE OF THE ABOVE CAPTIONED FIRM, CORPORATION OR BUSINESS, BY SUBMISSION OF THIS BID, AGREES AND CERTIFIES THE TRUTH AND ACCURACY OF ALL PROVISIONS OF THIS PROPOSAL, INCLUSIVE OF THE REQUIREMENTS, STATEMENTS, DECLARATIONS AND CERTIFICATIONS ABOVE AND IN THE SCHEDULE OF ITEMS AND PROPOSAL GUARANTY. EXECUTION AND SIGNATURE OF THIS FORM AND SUBMISSION OF THE SCHEDULE OF ITEMS AND PROPOSAL GUARANTY SHALL CONSTITUTE AN IRREVOCABLE AND LEGALLY BINDING OFFER BY THE BIDDER.

(Signature)

(Printed Name)

(Title)

(Date of Signature)

(Signature)

(Printed Name)

(Title)

(Date of Signature)

CONTRACTOR'S TOTAL BASE BID \$ _____

IT IS AGREED THAT THIS TOTAL, DETERMINED BY THE BIDDER, IS FOR PURPOSES OF OPENING AND READING BIDS ONLY, AND THAT THE LOW BID FOR THIS PROJECT WILL BE DETERMINED FROM THE EXTENSION AND TOTAL OF THE BID ITEMS BY DOTD.

CS-14AA
08/06

FOR INFORMATION ONLY